

# **JIAGANJ-AZIMGANJ MUNICIPALITY**

**Detailed Project Report for Construction of 1477 EWS Houses  
under  
BLC mode of Pradhan Mantri Awas Yojana (PMAY)-HFA (U) for  
Jiaganj-Azimganj Municipality  
2016-17**



**Jiaganj-Azimganj Municipality  
Dist: Murshdabad, West Bengal  
September, 2016**



## Table of Content

Introductory Note by Chairman .....	1
List of Tables .....	2-3
List of Figures and Maps.....	3
Abbreviations.....	3-4
 Working Definitions .....	 4-5
Brief Project Details .....	6-7
DPR Main Project.....	9-90
City Profile and Over view .....	10-17
Section 1: Introduction .....	18-19
Section 2: Salient features of HFAPoA and its linkage with proposed project and its justification.....	20-34
2.1 General introduction on status and Prioritization for proposed project.....	20
2.2. Summary of findings of HFAPoA. Physical infrastructure & Social infrastructure, Spatial, demographic and socio-economic profiles of slums/ Non slums; .....	20-30
2.3 Tenure Status .....	30 -31
2.4 Choice of Option/Vertical and its justification for housing and/or infrastructure .....	31-33
2.5 Resource mobilization strategy and Implementation strategy .....	34-35
Section 3: Project Concept and Scope	
3.1 Introduction of slum(s)/non Slum Area: .....	35-40
3.2. Location of slum(s) / non Slum Area, Tenural Status, Land use and Land Possession status.....	41-43
3.3. Existing basic infrastructure and its coverage .....	44-72
Section 4 – Description of Proposed Project and Planning	
4.1 Provision of Housing .....	73-88
4.2 Disaster Management and mitigation.....	88
4.3. Statutory approval including environmental clearance (as applicable)...	88-89
Section 5 – Project Cost Estimate	
5.1. Abstract cost estimates	
5.1.1 Component wise abstract for each slum/Non slums area.....	90-91
5.2. Detailed Estimates .....	92
5.2.1. Detailed Estimate of Provision of Housing .....	92-112
5.2.2. Detailed Estimate of adoption of concrete Road .....	114-115

## **Section 6 – Project Implementation & Management Framework**

<b>6.1. Institutional Framework for implementation .....</b>	<b>113</b>
<b>6.2. Implementation schedule .....</b>	<b>113-114</b>
<b>6.3. Quarterly component wise investment schedule vis-a-vis means of finance (Central/State/ULB/Beneficiaries share).....</b>	<b>114</b>
<b>6.4. Monitoring mechanism at State, ULB and Community level. ....</b>	<b>114</b>
<b>6.5. Quality Control &amp; Quality Assurance Plan.....</b>	<b>114</b>
<b>Section 7 – Operation &amp; Maintenance Plan.....</b>	<b>114</b>
<b>Section 8 – Project Financials.....</b>	<b>115</b>

**Annexure-1: Annexure of Drawings**

**Annexure-2: Annexure of Slums and Non slums Map**

**Annexure-3: Annexure of Slums and Non slums Photos**

**Annexure-4: Annexure of Slums and Non slums Beneficiary List**

**Annexure-5: Annexure of BoC Resolution**



## Executive Summary

### Project Details

1	State					West Bengal
2	City					Jiaganj-Azinganj Municipality
3	Project Name					Pradhan Mantri Awas Yojana , Housing For All. (Urban)
4	Project Cost			(Rs. In Lacs)		5978.90
5	Central Share			(Rs. In Lacs)		2215.50
6	State Share			(Rs. In Lacs)		3122.38
7	ULB Share			(Rs. In Lacs)		271.77
8	Beneficiary Share			(Rs. In Lacs)		369.25
10	SOR Adopted					PWD (WB) w.e.f 1.7.14 with current corrigendum.

### Project Contributions (Physical + Financial) (Rs. In lacs)

Sl No.	Scheme Component	Type	Quantity	Unit	Rate (in Rs./unit)	Proposed project cost (in lakh)	Appraised Project Cost (in lakh)	Central Share	State Govt. Share	ULB Share	Beneficiaries Share
A. HOUSING											
1	New in- situ										
	Single storied units		1477	Nos.	368000	5435.36	5435.36	2215.50	2850.61		369.25
2	Up-gradation										
3	Rental										
4	Transit										
	Total Housing Cost Sub Total (A)					5435.36	5435.36	2215.50	2850.61	0.00	369.25
B. INFRASTRUCTURE											
1	Roads										
i	CC Roads	2.5 m wide	13267	Mtr	4097	543.54	543.54	0.00	271.77	271.77	
iii	Interlocking Block										
iv	Culverts										
2	Water Supply										
i	UGSR										

*[Signature]*  
Chairman



Sl No.	Scheme Component	Type	Quantity	Unit	Rate (in Rs./unit)	Proposed project cost (in lakh)	Appraised Project Cost (in lakh)	Central Share	State Govt. Share	ULB Share	Beneficiaries Share
ii	SR										
i	Internal Pipeline	NA	0	Nos.	1572	0	0	0	0	0	
iv	Pump Station & tube well										
Total Infrastructure Cost Sub Total (B)											
Total (A+B)						543.54	843.54	2315.50	271.77	271.77	0.00
						5978.90	5978.90		3122.38	271.77	369.25

*Aman*

Signature of the ULB Level Competent

Name & Designation: Ataur Rahaman

Address: Jiaganj, Dist. - Murshidabad

Fax No: + 91-33-

Telephone No.: 03483-253222

Mobile No.: 9474077120

E-mail: ataur\_rahaman@rediffmail.com

*[Signature]*

Signature of the State Level Competent Technical Officer

Name & Designation: Anrit Das, Chief Engineer, Municipal Engineering Dto, Govt. of West Bengal

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Signature of the State Level Nodal Officer

Name & Designation: Sri M.N. Pradhan, IAS  
Director, SUDA

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**Annexure-7C**  
**(Para 14.5 of the Guidelines)**

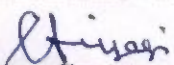
**Format for Project under Beneficiary led Construction or Enhancement**

1	Name of the State	:	WEST BENGAL						
2	Name of the City	:	JIAGANJ-AZIMGANJ						
3	Project Name	:	HOUSING FOR ALL						
4	Project Code	:							
5	State Level Nodal Agency	:	SUDA						
6	Implementing Agency / ULB	:	JIAGANJ-AZIMGANJ MUNICIPALITY						
7	Date of approval by State Level sanctioning and Monitoring Committee (SLSMC)	:							
8	No. of locations covered in project	:	Name of Location	No. of beneficiary	Whether Slum (Y/n)	if yes, 1 if notified, 2 if recognised and 3 if identified		if slum, whether it gets completely rehabilitated Y/n	
		:	JIAGANJ-AZIMGANJ	1477	Y	1		N	
9	Project Cost (In Rs. Lakh)	:	5435.36						
10	No of Beneficiaries covered in the project	:	Gen	SC	ST	OBC	Total	Minority	Person with Disability
		:	849	567	31	30	1477	123	1
11	Whether beneficiary have been selected as PMAY guideline? (Yes/No)	:							
12	No. of houses constructed / acquired Please specify ownership (any of these)	:	Joint (01) = 0	Female (02) = 0	Male (03) = 0	Transgender (4) = 0			
13	No. of beneficiaries covered in project	:	Male (01) = 897	Female(02) = 579	Transgender (03) = Nil				
14	Whether it has been ensured that selected beneficiaries have rightful ownership of the land	:	Yes						
15	Whether building plan for all houses have been approved	:	-						
16	i) Govt grant required (Rs. 1.5 lakh per eligible beneficiary) (Rs in Lakhs)	:	2215.5						
	ii) State grant, if any (Rs. in lakhs)	:	2850.61						
	iii) ULB grant, if any (Rs. in Lakh)	:	N.A.						
	iv) Beneficiary Share (Rs. in lakhs)	:	369.25						
	Total (Rs. in lakh)	:	5435.36						
17	Whether technical specification/ design for housing have been ensured as per Indian Standards/NBC/ State norms.	:	Yes						
18	Whether it has been ensured that balance cost of construction is tied up with State grant, ULB grant & beneficiary share?	:	Yes						
19	Whether trunk and line infrastructure is existing or being provisioned	:	Yes						
	i) Water Supply (Yes/ No)	:	No						
	ii) Sewerage (Yes / No)	:	Yes						
	iii) Road (Yes / No)	:	Yes						
	iv) Storm Water Drain (yes/no)	:	Yes						
	v) External Electrification (Yes/No)	:	Yes						
	vi) Solid Waste Management (Yes/No)	:	N.A.						
	vii) Any other, specify	:	N.A.						
	viii) In case, any infirmary has not been proposed, reasons thereof.	:	N.A.						
20	Whether disaster (earthquake, flood, cyclone landslide etc.) resistance features have been adopted in concept, design and implementation?	:	Yes						
21	Whether Demand Survey completed for entire city?	:	Yes						
22	Whether city-wide integrated project have been formulated? If not, reasons thereof.	:	Yes						
23	Whether validation with SECC data for housing conditions conducted?	:	Yes						
24	Whether Direct Benefit Transfer (DBT) of fund to individual bank account of beneficiary ensured in the project?	:	Yes						
25	Whether there is provision in DPR for tracking/ monitoring the progress of individual houses through geo-tagged photographs?	:	Yes						
26	Whether any innovation/cost effective/Green technology adopted in the project?	:	Yes						
27	Comments of SLAC after techno economic appraisal of DPR.	:							
28	Brief of project , including any other information ULB/State would like to furnish.	:							

\*State will give code number to each project sanctioned under HFA as 'ABCDEF GHIJKLM'

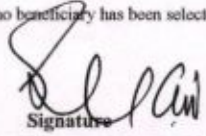
(Where 'AB' is State Code as per census 'CDEFGH' is city code as per census 'IJ' is running number of project of the city and 'K' is project component code. 'K' will be 1 for in-situ- slum redevelopment, 2-for Relocation 3- for AHP and 4-for Beneficiary led-Construction or enhancement), 'L' will be N- for New, R- for Revised, M will be running number which will be O for new and 1 and so on for revision.

It is hereby confirmed that State /UT/ and ULB have checked all the beneficiaries as per guideline of HFA. It is also submitted that no beneficiary has been selected for more than one benefit under the Mission including Credit Linked Subsidy Scheme (CLSS) component of the Mission.

  
Signature  
(State Level Nodal Officer)

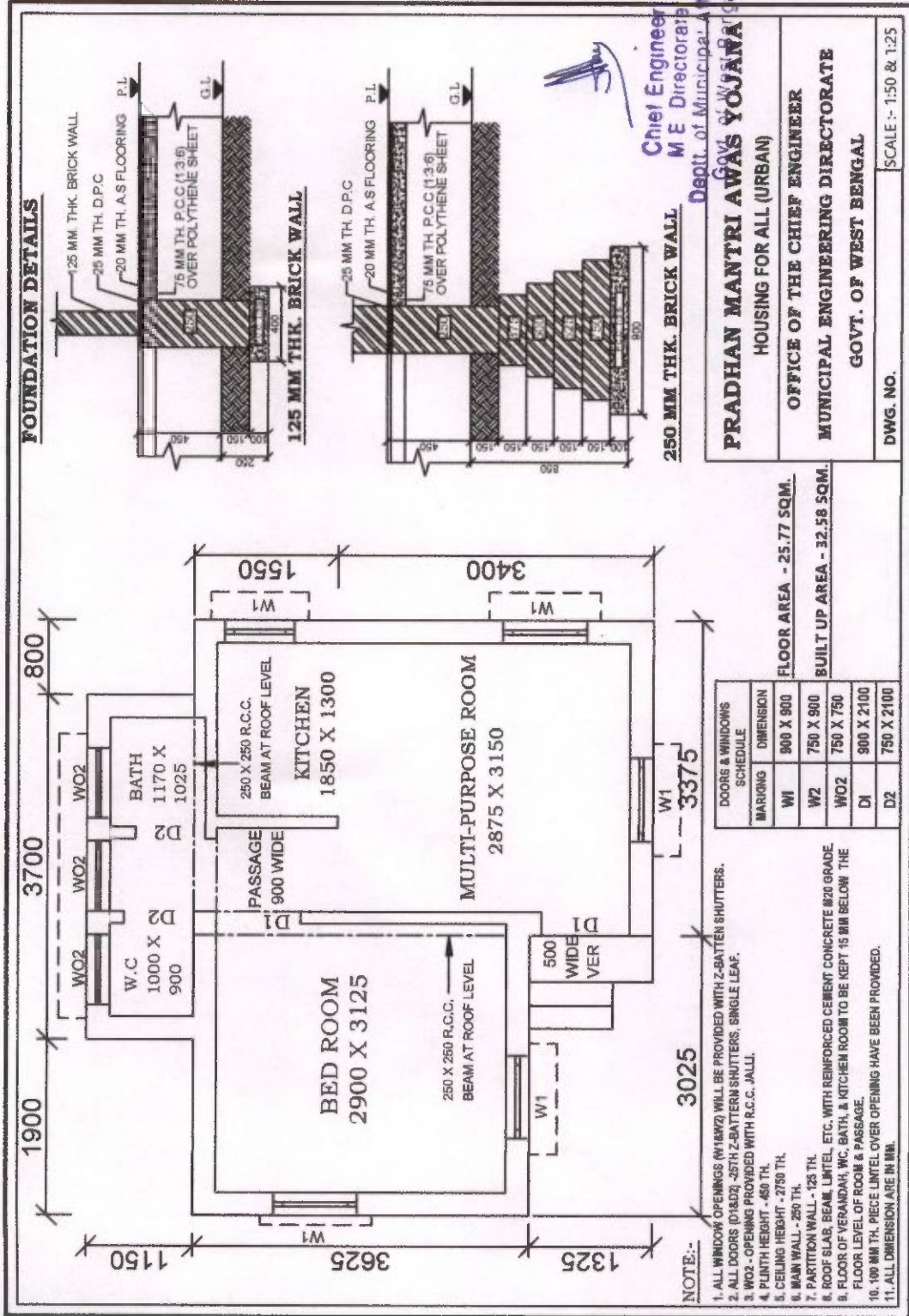
Nodal Officer  
&  
Urban Planner  
Jiaganj-Azimganj Municipality

(Secretary / Principal Secretary, Concerned Department)

  
Signature  
Chairman  
Sankar Mandal  
Jiaganj-Azimganj Municipality

Chief Engineer  
M.E Directorate  
Dept. of Municipal Affairs  
Govt. of West Bengal





*S. K. Choudhury*  
Chairman  
Jiaganj-Azimganj Municipality

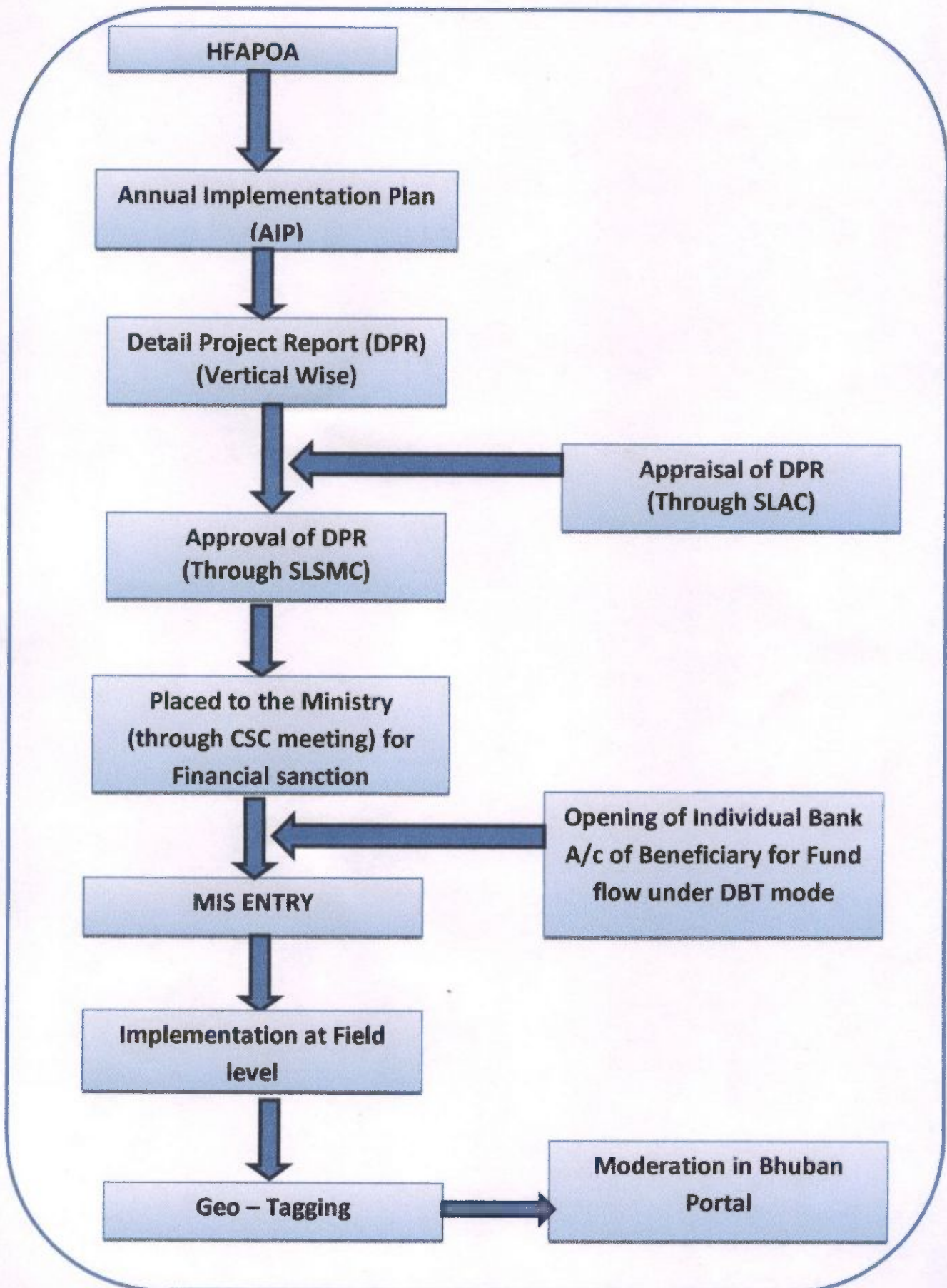
# Implementation Schedule 2016-17

Sl. No.	Activity	2016												2017											
		1st	2nd	3rd	4th	1st	2nd	3rd	4th	1st	2nd	3rd	4th	1st	2nd	3rd	4th	1st	2nd	3rd	4th	1st	2nd	3rd	4th
1.0	Preparation of test works & M&E entry																								
2.0	Construction of Single storied DU including S & P, Elec.																								
3.0	Geotagging of DU																								
4.0	Infrastructure Works, (streaming, formwork and foundation for field works)																								

  
 Chairman  
 Jagang-Azinganj Municipality



## Work flow of PMAY – HFA (U) for 2016-17



**PMAY SURVEY DATE AND DATE OF ORIENTATION CAMPS**

- 16.09.2015 At 5 pm at Bhagirathi PTTI Hall Orientation of the Councillors
- 17.09.2015 At 4 pm at a ground in front of House of Ashini Biswas for Ward No. 7, 8 and 16
- 19.09.2015 At 4 pm at Sukantapally ground for Ward No. 3, 5 and 15
- 19.09.2015 At 6 pm at the Sripat Sing College New Building Hall for Ward No. 1, 2 and 6
- 20.09.2015 At 2 pm at Jiaganj Engineering College for Ward No. 4, 9,10 and 11
- 20.09.2015 At 6 pm at Azimganj Mahamandal for Ward No. 12, 13, 14 and 17
  
- **Demand Survey for PMAY Started on 17.09.2015 and completed on 29.09.2015**



## **PREFACE**

PradhanMantriAwasYojana (PMAY) aims at Providing Housing for All (HFA) by 2022 when the Nation Complete 75 years of its independence.

The urban homeless persons contribute to the economy of the cities and thus the Nation as cheap labour in the informal sector; yet they live with no shelter or social security. The urban homeless service with many challenges like no access to elementary Public Services such as health, education, food, water and sanitation. PradhanMantriAwasYojana (PMAY) also aims at providing a pucca house to every family with water connection, toilet facilities, 24 X 7 electricity supply and access.

The Mission seeks to address the housing requirement of urban poor including slum dwellers through "In Situ" Slum Redevelopment, Affordable Housing through credit linked subsidy, and Affordable Housing in partnership and subsidy for beneficiary led individual house. Under the mission, beneficiaries can take advantage under one component only.

Total beneficiaries of the scheme are 877 Nos. from 63 Nos. slum and 600 Nos. of Non Slum projected for the year 2016-17.

Total cost of the project is **Rs. 5978.90 lakhs** as per relevant department & P.W.D. schedule of rates.

## Introductory Note by Chairman

A roof over the head is a basic necessity. A contrite roof is a dream of every common people. The programme, 'Housing for All', is an opportunity to all from poor to middle class people who dream for a 'Pucca' House but could not achieve because of financial crisis.



Our twin town municipality of Jiaganj-Azimganj is one of such uncommon small municipal towns in the State of West Bengal where river flows in between and there is no road or rail connectivity. There is no major industry or government or private establishment in the municipal area. In absence of these the people are mostly dependent on petty business activities. A large number of people live in a house having no 'Pucca' roof over their head. Like all other places the problem of encroachment on government land or unclaimed private land is a major problem.

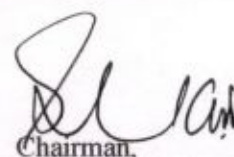
Urbanisation and its related effect attract people to the town. As a result of which slums are growing thick and fast. The people in these twin towns have migrated from adjoining villages or from neighbouring country Bangladesh. These people have started to live in a 'Kutchha' house and are engaged in labour works. Over the passage of time 63 slum areas have grown up in this twin town. Slums are the source of labour in every city which cannot be ignored. Therefore a measure should be taken to improve the living condition of those slum dwellers.

'Housing For All' under Primary Minister Awas Yojana is an opportunity for all of us who dream of a developed city. We vow to implement this project with its all opportunities for the betterment of life of common citizens.

The Jiaganj-Azimganj Municipality from the very first day have taken initiative to implement the programme to the best of its ability. The Municipality has organised orientation camps for the Councillors as well as at the Ward level camps to make the people aware of the programme. Training of the enumerators and Supervisors have also been organised without any wastage of time. As a result of which we have completed the demand Survey form all wards.

Thus we have planned to achieve our target of providing 'Pucca' houses to all who have not the 'Pucca' house till today. Moreover the programme has provided an opportunity to the middle class people to make a house of their own by availing the 'Credit Linked-Subsidy' scheme. The people living in Slum or in Non-Slum area who have own land but do not have 'Pucca' house of his own can also built a 'Pucca' house for his/her through the 'Beneficiary Led-Construction' Scheme. The municipality has also assessed the number of beneficiaries who have neither own land nor any 'Pucca' house of his or her own. Such beneficiaries will be provided houses at considerable low rate though 'Affordable Housing in Partnership'. The municipality has also pointed out some slum pockets where the 'In-City Slum Redevelopment' scheme can be implemented with the assistance of Private Partners. Thus a comprehensive development plan covering all Slums and Non-Slum area where people require 'Pucca' houses.

The Municipality has prepared this DPR, 2016-17 for implementation of Prime Minister Aways Yojana in the municipal area.



Chairman,  
Jiaganj-Azimganj Municipality  
Chairman  
Jiaganj-Azimganj Municipality



## List of Tables

Sl. No	Name of the Table
1	Table-1: Format for Projects under Beneficiary led Construction
2	Table-2: City at a Glance
3	Table-3: Proposed Houses in slum area
4	Table-4: Proposed Houses in non slum area
5	Table- 5: Existing Land Use Pattern of Jiaganj-Azimganj Municipal Area
6	Table No-6: Water Supply
7	Table No-7: Distribution Network for water supply
8	Table No-8: Sewerage
9	Table No-9: Storm and Sullage Water Drains Collection network
10	Table No-10: Road Network
11	Table-11: Collection vehicles bins / containers
12	Table-12: Summary of Public Consultation and Need Assessment:
13	Table-13: Justification of the Project
14	Table-14: Slum-wise Intervention strategies for Tenable Slums
15	Table-15: Year-wise Proposed Interventions for Other Urban Poor based on demand survey
16	Table-16: Introduction of slum(s)/non Slum Area
17	Table-17: Non Slum Area
18	Table-18: Location of slum(s) / non Slum Area, Tenure Status, Land use and Land Possession status
19	Table-19: The project slums and existing scenario of infrastructure
20	Table-20: Social Infrastructure at a glance
21	Table-21: Dwelling units
22	Table-22: Share of Fund
23	Table-23: Statutory approval including environmental clearance
24	Table-24: Component wise abstract for each slums and non-slums area
25	Table-25: Detailed Estimate of Provision of Housing

26	Table-26: ESTIMATE FOR ELECTRICAL WORKS FOR ONE DWELLING UNIT UNDER PMAY
27	Table-27: Cost Estimate for 2 Nos Leach Pit for single unit Dwelling Unit
28	Table-28: Detailed Estimate for Single Dwelling unit
29	Table-29: Detailed Estimate of adoption of technology for Concrete
30	Table-30: Detailed Estimate of adoption of technology for Water Connection
31	Table-31: Quarterly component wise investment schedule vis-a-vis means of finance (Central/State/ULB/Beneficiaries share)
32	Table-32: Project Financials

#### List of Figures

Sl.No	Name of the Figure
1	Figure-1: Resource mobilization strategy and Implementation strategy

#### List of Maps

Sl. No	Name of the Maps
1	All Slum showing in Map
2	Non Slum Map

#### Abbreviation

A&OE	Administrative and Other Expenses	MoA	Memorandum of Agreement
AHP	Affordable Housing in Partnership	MoHUPA	Ministry of Housing and Urban Poverty Alleviation
AIP	Annual Implementation Plan	MoU	Memorandum of Understanding
CDP	City Development Plan	IIT	Indian Institute of Technology
CLS	Credit linked subsidy	NA	Non Agricultural (NA)
CNA	Central Nodal Agencies	NBC	National Building Code
CSMC	Central Sanctioning and Monitoring Committee	NHB	National Housing Bank
DIPP	Department of Industrial Policy and Promotion	NOC	No Objection Certificate
		NPV	Net Present Value
DPR	Detailed Project Report	PLI	Primary Lending Institution
EMI	Equated Monthly Instalment	SFCPoA	Slum Free City Plan of Action
EWS	Economically Weaker Section	SLAC	State Level Appraisal Committee



FAR	Floor Area Ratio	SLNA	State level Nodal Agencies
FSI	Floor Space Index	SLSMC	State Level Sanctioning and Monitoring Committee
HFA	Housing for All		
HFAPoA	Housing for All Plan of Action	TDR	Transfer of Development Rights
HUDCO	Housing and Urban Development Corporation	TPQMA	Third Party Quality Monitoring Agency
IEC	Information Education & Communication	ULB	Urban Local Body
IFD	Integrated Finance Division	UT	Union Territory
LIG	Low Income Group	MD	Mission Directorate

### Working Definitions

Affordable Housing Project:	Housing projects where 35% of the houses are constructed for EWS category
Beneficiary	<p>A beneficiary family will comprise husband, wife and unmarried children.</p> <p>The beneficiary family should not own a pucca house (an all weather dwelling unit) either in his/her name or in the name of any member of his/her family in any part of India.</p>
Carpet Area	Area enclosed within the walls, actual area to lay the carpet. This area does not include the thickness of the inner walls
Central Nodal Agencies	Nodal Agencies identified by Ministry for the purposes of implementation of Credit linked subsidy component of the mission
Economically Weaker Section (EWS):	EWS households are defined as households having an annual income up to Rs. 3,00,000 (Rupees Three Lakhs). States/UTs shall have the flexibility to redefine the annual income criteria as per local conditions in consultation with the Centre.
EWS House	An all weather single unit or a unit in a multi-storeyed super structure having carpet area of upto 30 sq. m. with adequate basic civic services and infrastructure services like toilet, water, electricity etc. States can determine the area of EWS as per their local needs with information to Ministry.
“Floor Area Ratio” (FAR)/FSI	<p>The quotient obtained by dividing the total covered area (plinth area) on all the floors by the area of the plot:</p> $\text{FAR} = \frac{\text{Total covered area on all the floors} \times 100}{\text{Plot area}}$ <p>If States/Cities have some variations in this definition, State/City definitions will be accepted under the mission</p>
Implementing	Implementing agencies are the agencies such as Urban Local Bodies, Development Authorities, Housing Boards etc. which are selected by State Government/SLSMC

Agencies	for implementing Housing for All Mission.
Low Income Group (LIG):	LIG households are defined as households having an annual income between Rs.3,00,001 (Rupees Three Lakhs One) up to Rs.6,00,000 (Rupees Six Lakhs). States/UTs shall have the flexibility to redefine the annual income criteria as per local conditions in consultation with the Centre.
Primary Lending Institutions (PLI)	Scheduled Commercial Banks, Housing Finance Companies, Regional Rural Banks (RRBs), State Cooperative Banks, Urban Cooperative Banks or any other institutions as may be identified by the Ministry
Slum	A compact area of at least 300 population or about 60-70 households of poorly built congested tenements, in unhygienic environment usually with inadequate infrastructure and lacking in proper sanitary and drinking water facilities.
State Land Nodal Agencies (SLNAs)	Nodal Agency designated by the State Governments for implementing the Mission
Transfer of Development Rights (TDR)	TDR means making available certain amount of additional built up area in lieu of the area relinquished or surrendered by the owner of the land, so that he can use extra built up area himself in some other land.



## **Brief Project Details**

Pradhan Mantri Awas Yojana (PMAY) aims at Providing Housing for All (HFA) by 2022 when the Nation Complete 75 years of its independence.

The urban homeless persons contribute to the economy of the cities and thus the Nation as cheap labour in the informal sector; yet they live with no shelter or social security. The urban homeless people are not getting service with many challenges like no access to elementary Public Services such as health, education, food, water and sanitation. Pradhan Mantri Awas Yojana (PMAY) also aims at providing a pucca house to every family with water connection, toilet facilities, 24 X 7 electricity supply and access.

The Mission seeks to address the housing requirement of urban poor including slum dwellers through “In Situ” Slum Redevelopment, Affordable Housing through credit linked subsidy, and Affordable Housing in partnership and subsidy for beneficiary led individual house. Under the mission, beneficiaries can take advantage under one component only.

Total beneficiaries of the scheme are 877 Nos from 63 Nos. of slum and 600 Nos. in non slums area under the projected for the year 2016-17.

Total cost of the project is **Rs. 5978.90 lakhs** as per relevant department & P.W.D. schedule of rates.

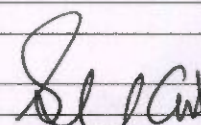
## Annexure 7C

Table-1: Format for Projects under Beneficiary led Construction

## (Para 14.5 of the Guidelines)

## Format for Project under Beneficiary led Construction or Enhancement

1	Name of the State	:	WEST BENGAL					
2	Name of the City	:	JIAGANJ-AZIMGANJ					
3	Project Name	:	HOUSING FOR ALL					
4	Project Code	:						
5	State Level Nodal Agency	:	SUDA					
6	Implementing Agency / ULB	:	JIAGANJ-AZIMGANJ MUNICIPALITY					
7	Date of approval by State Level sanctioning and Monitoring Committee (SLSMC)	:						
8	No. of locations covered in project	:	Name of Location	No. of beneficiary	Whether Slum (Y/n)	if yes, 1 if notified, 2 if recognised and 3 if identified	if slum, whether it gets completely rehabilitated Y/n	
		:	JIAGANJ-AZIMGANJ MUNICIPALITY	1477	Y	1	N	
9	Project Cost (In Rs. Lakh)	:	5435.36					
10	No of Beneficiaries covered in the project	:	Gen	SC	ST	OBC	Total	Minority
		:	849	567	31	30	1477	123
11	Whether beneficiary have been selected as PMAY guideline? (Yes/No)	:						
12	No. of houses constructed / acquired Please specify ownership (any of these)	:	Joint (01) = 0	Female (02) = 0	Male (03) = 0	Transgender (4) = 0		
13	No. of beneficiaries covered in project	:	Male (01) = 897	Female(02) = 579	Transgender (03) = Nil			
14	Whether it has been ensured that selected beneficiaries have rightful ownership of the land	:	Yes					
15	Whether building plan for all houses have been approved	:						
16	i) GoI grant required (Rs. 1.5 lakh per eligible beneficiary) (Rs in Lakhs)	:	2215.5					
	ii) State grant, if any (Rs. In lakhs)	:	2850.61					
	iii) ULB grant, if any (Rs. In Lakh)	:						
	iv) Beneficiary Share (Rs.in lakhs)	:	369.25					
	Total (Rs.in lakh)	:	5435.36					
17	Whether technical specification/ design for housing have been ensured as per Indian Standards/NBC/ State norms.	:	Yes					
18	Whether it has been ensured that balance cost of construction is tied up with State grant, ULB grant & beneficiary share?	:	Yes					
19	Whether trunk and line infrastructure is existing or being provisioned	:	Yes					
	i) Water Supply (Yes/No)	:	Yes					
	ii) Sewerage (Yes / No)	:	No					
	iii) Road (Yes / No)	:	Yes					
	iv) Storm Water Drain (yes/No)	:	Yes					
	v) External Electrification (Yes/No)	:	Yes					
	vi) Solid Waste Management (Yes/No)	:	Yes					
	vii) Any other, specify	:						



Chairman

Jiaganj-Azimganj Municipality

PMAY (Housing for All)



	viii) In case, any infuture has not been proposed, reasons thereof.	:	
20	Whether disaster (earthquake, flood, cyclone landslide etc.) resistance features have been adopted in concept, design and implementation?	:	Yes
21	Whether Demand Survey completed for entire city?	:	Yes
22	Whether city-wide integrated project have been formulated? If not, reasons thereof.	:	Yes
23	Whether validation with SECC data for housing conditions conducted?	:	Yes
24	Whether Direct Benefit Transfer (DBT) of fund to individual bank account of beneficiary ensured in the project?	:	Yes
25	Whether there is provision in DPR for tracking/ monitoring the progress of individual houses through geo-tagged photographs?	:	Yes
26	Whether any innovation/cost effective/Green technology adopted in the project?	:	Yes
27	Comments of SLAC after techno economic appraisal of DPR.	:	
28	Brief of project, including any other information ULB/State would like to furnish.	:	

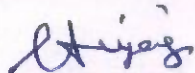
\*State will give code number to each project sanctioned under HFA as 'ABCDEFGHIJKLM'

(Where 'AB' is State Code as per census 'CDEFGH' is city code as per census 'IJ' is running number of project of the city and 'K' is project component code i.e. 'k' will be 1-for in-situ- slum redevelopment, 2 -for Relocation 3- for AHP and 4-for Beneficiary led- Construction or enhancement), 'L' will be N- for New, R- for Revised, 'M' will be running number which will be O for new and 1 and so on for revision.

It is hereby confirmed that State /UT/ and ULB have checked all the beneficiaries as per guideline of HFA. It is also submitted that no beneficiary has been selected for more than one benefit under the Mission including Credit Linked Subsidy Scheme (CLSS) component of the Mission.

**Signature**

(ULB Nodal Officer)



Nodal Officer

&

Urban Planner

Jiaganj-Azimganj Municipality

**Signature**

(Chairman/Chairperson)  
Chairman  
Jiaganj-Azimganj Municipality

# **DPR Main Report**



## **City Profile and Overview**

### **History**

Jiaganj-Azimganj Municipality consists of two cities namely Jiaganj and Azimganj situated on both sides of the river Bhagirathi. Since inception this twin city is a conglomeration of many temples, mosques and Gurdowaras. From very olden days this cities are considered to be holy place for different religious groups' viz. Janis, Shiks and Buddhists. This city is a glaring example of secularism where many different religious groups namely Hindus, Muslims, Jains, Shiks, Christians and Buddhists are living harmoniously since time immemorial.

Earlier this municipality was a part of Murshidabad municipality. Since 1896 it became a separate municipality. The municipality is well connected by the Berhampore-Lalgola Highway which runs through the eastern boundary of the town. This road is also well connected with the National Highway – NH 34. The two major railway lines, namely Lalgola-Sealdah and Howrah-Malda via Azimganj are also passing through the municipality. The Jiaganj railway station is located on the Jiaganj (Lalgola-Sealdah) and Azimganj station is located at Azimganj (Howrah-Malda via Azimganj). River Bhagirathi is the focal point of these twin city. 24 hours fari service is provided by boat men.

Rani Bhabani's terracotta temple which is considered to be one of the best examples of Bengal's terracotta art is closed to the Municipal area. There are also few Jain temples in the municipal area.

### **Administrative Boundaries**

Jiaganj-Azimganj Municipality consists of two town of Jiaganj and Azimganj. The twin town is situated on the opposite banks of the river Bhagirathi, a branch of river Ganges. Jiaganj (24.23°N 88.27°E) is on the east side while Azimganj (24.23°N 88.25°E) is on the west. The municipal area is surrounded by Murshidabad town in the south, vast land of Bhagabangolla in the North and while in the east and west sides by the Rada area of Nabagram and Sagardighi. This conglomeration of ancient Gauda (North Bengal) and Rada area constituting this municipality was a part of Murshidabad municipality till 1896 when it was curved out as a separate municipal area.

### **Linkage**

Earlier this municipality was a part of Murshidabad municipality. Since 1896 it became a separate municipality. The municipality is well connected by the Berhampore-Lalgola Highway which runs through the eastern boundary of the town. This road is also well connected with the National Highway – NH 34. The two major railway lines, namely Lalgola-Sealdah and Howrah-Malda via Azimganj are also passing through the municipality. The Jiaganj railway station is located on the Jiaganj (Lalgola-Sealdah) and Azimganj station is located at Azimganj (Howrah-Malda via

Azimganj). River Bhagirathi is the focal point of these twin city. 24 hours ferry service is provided by boat men.

There are two parallel railway links on both the sides of river Bhagirathi. One is connecting Sealdah to Jiaganj through Lalgola Branch Line and another Howrah to Azimganj through Barharwa-Azimganj-Katwa Loop Line. A new rail bridge is coming up over river Bhagirathi connecting Nashipur to Azimganj. Azimganj-Nalhati Railway line started in the year 1872.

Apart from this through road links Jiaganj is connected to adjacent townships like Lalbagh (Murshidabad), Berhampore, Lalgola, Bhagabangola. Jiaganj and Azimganj are connected by boat service managed by local municipality on river Bhagirathi.

### Economic Base

The economic base of a town is mostly dependent on the major establishments of the town as well as the surrounding area. The main activity of the major portion of the citizen is directly related to the economic strength of the said town. Instead of large scale industries there are a good many nos. of small scale industries, clustered in the town. The main economic activity of this area is petty business.

Azimganj has a tropical climate. The average annual temperature in Azimganj is 26.2 °C. The average annual rainfall is 1342 mm. The warmest month of the year is May with an average temperature of 30.5 °C. In January, the average temperature is 18.5 °C. It is the lowest average temperature of the whole year.

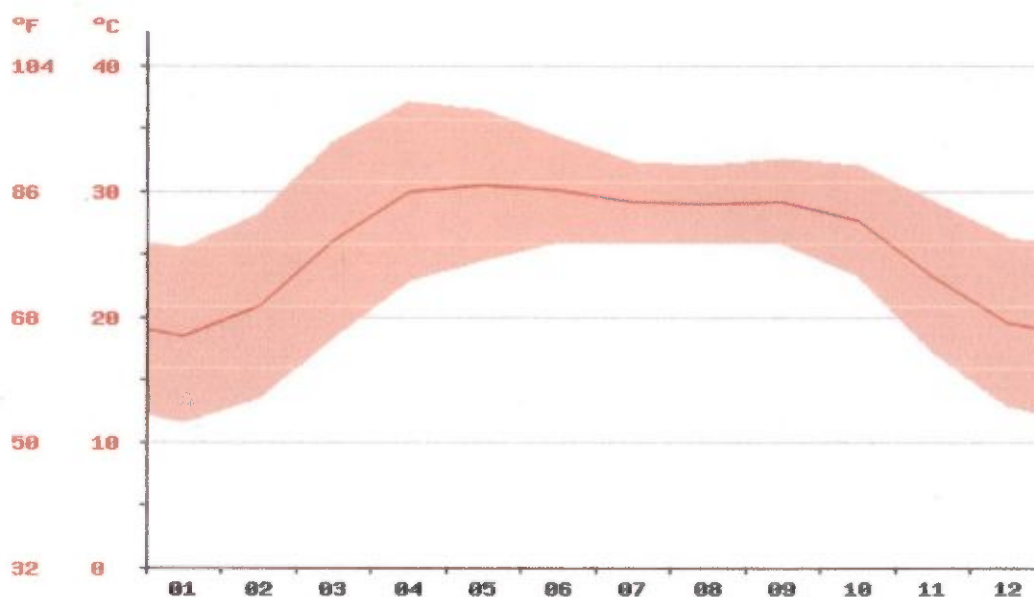


Figure: 1 Climate



## 2.11: SOIL AND GROUND WATER SCENARIO

The land of Jiaganj and Azimganj is very fertilized and rich. The land is constructed by sedimentary rocks. The land is high, slightly undulating, but is interspersed with numerous swamps and beds of old rivers. The soil is greyish and reddish, rich in lime and iron oxide.

### Demographic Growth & Population Project

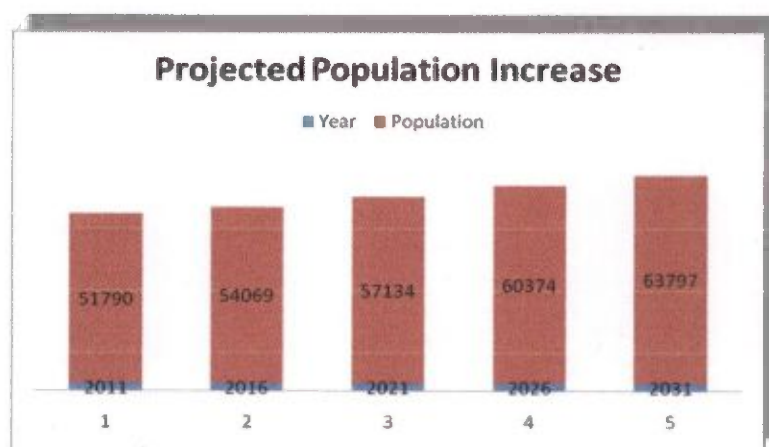
### Demographic Growth & Population Projection

In the municipality there are no major industries and basically it's a residential town. A considerable number of people (mainly within the ward 1, 3, 8, 12 and 15) are dependent on agricultural activities and again a large number of people are dependent on business and other commercial activity and considerable number of people (mainly within the ward No. 1, 7, 9 13, 14 and 16) are dependent on household industries.

According to the 1991 census annual average growth rate of population in the ULB is around 2.13 percent. According to the 2001 census report the total population in the municipality is 47,212. Projected population growth and density trend are given in following table and graph.

**Table 3: Projected Population & Density**

	2011	2016	2021	2026	2031
<b>Population</b>	<b>51790</b>	<b>54069</b>	<b>57134</b>	<b>60374</b>	<b>63797</b>
<b>Density (person/ sq km)</b>	4441.68	4637.14	4900.00	5177.87	5471.44



**Figure 2 :: Projected population increase in the municipality**

There was a spurt in population at the time of partition when a large number of people intrude and settled down at the fringe of the municipal area, mainly at the eastern part of the municipality, besides the Bhagirathi River. Gradually, a number of such refugee settlements grew up which are represented as slums pockets of the municipal area. Over time settlers got the ownership of their land through distribution of Patta from the State Government.

To meet the requirement of urban services for the increasing population a major up gradation of the services is required. In the past an integrated municipal system and could not be considered in absence of lack of fund for municipal development in West Bengal. Only some piecemeal crisis management was the prime activity of this municipality. Also, a considerable time was necessary to achieve the infrastructure for providing this required level of municipal services, which reached now at an acme to provide swift and efficient municipal administration & services.

Ward wise distribution of population as per Census 2001 and 2011 is given below:

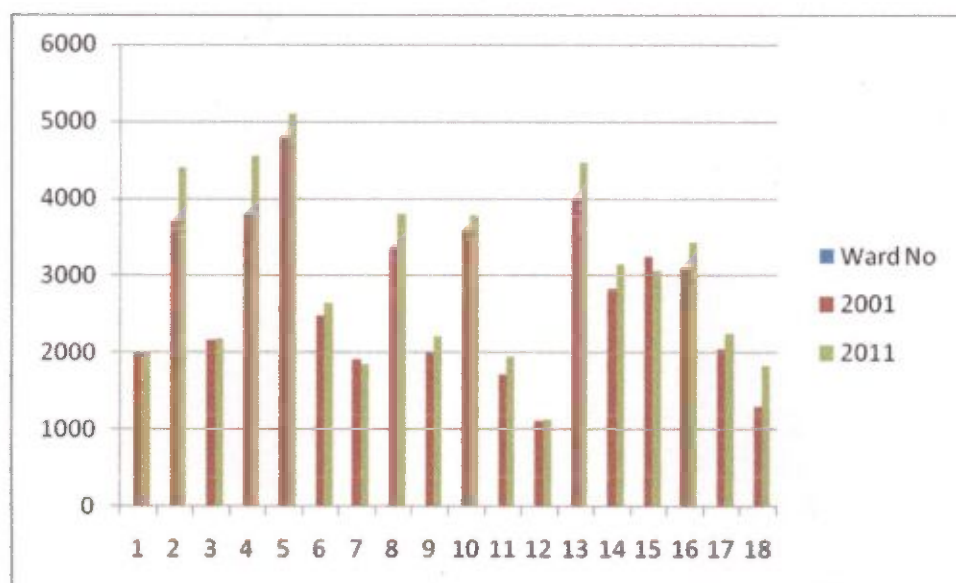


Figure 3.: Ward wise distribution of population

The town has **63 Slum** pockets in total out of which 59 slums are notified and 4 are Non-Notified. The municipal authority before preparation of Housing for All Plan of Action has been carried out a Physical and Socio-economic Survey for determination of conditions of all Slum pockets and to assess priority on the basis of the Demand Survey Report.

Table-2: City at a Glance

1	Name of the District :	Murshidabad
2	Year of establishment :	1896
3	Area (in sq. Km) :	11.66
4	No. of wards :	17
5	Distance from District Headquarter :	20 km.
6	Population (census 2011) :	
6.1	Male	26112
6.2	Female	25678
6.3	Total	51790
7	Density of Population (Per sq. km.) :	4441.68
8	Break up of Population (2011) :	
8.1	Scheduled Caste	14562
8.2	Scheduled Tribe	431



8.3	Minorities	No Data Available with the municipality
9	<b>Date when last election held</b>	2010
10	<b>Assessment of Property:</b>	
10.1	Total holdings	12011
10.2	Total no. of holdings whose assessment has been done	12011
10.3	No. of holdings to whom demand notice are issued	12011
10.4	Total demand for 2013-14	Rs. 157.69 lacks
10.5	Total Collection for 2013-14	Rs. 36.74 lacks
10.6	Year of Last assessment by West Bengal Valuation Board	2011
10.7	Year / quarter of Imposition of current Property Tax	Not yet Imposed
11	<b>Literacy :</b>	
11.1	Male	20022
11.2	Female	17706
11.3	Total	37728
11.4	Percentage of Literate Population(2011)	72.85
12	<b>Number of BPL Household (as per SUDA Survey) :</b>	7076
13	<b>Scenario of Slum :</b>	
13.1	Total No. of Slum	63
13.2	Total Slum Population (as per USHA survey)	26304
13.3	Percentage of Slum Population to the total population	50.79
13.4	No. of Slum where Slum Infrastructure Improvement sanctioned under BSUP/ IHSDP	51
13.5	No. of Slum where Slum Infrastructure Improvement already done under BSUP/ IHSDP-	26
14	<b>Housing status for Urban Poor :( as on 31.03.2014)</b>	
14.1	No. of dwelling units targeted to be provided under BSUP/ IHSDP	1114
14.2	No. of beneficiaries already provided with Houses under BSUP/ IHSDP	874
14.3	No. of beneficiaries provided with Houses under " Housing for Urban Poor "	12
15	<b>Road :</b>	
15.1	Length of Metalled Road (in km.)	96.46
15.2	Length of Non-Metalled Road (in km.)	19.74
15.3	Length of other Roads (in km.)	
15.4	Total length of Road (in km.)	116.20
15.5	Total no. of wards fully covered with Metal / Cement Concrete Road	5
16	<b>Drainage :</b>	
16.1	Length of Kutchha Drain (in km.)	1.05
16.2	Length of Pucca Drain (in km.)	45.39
16.3	Length of underground / covered Drain (in km.)	Nil
16.4	Total length of Drain (in km.)	46.44

16.5	No. of wards fully covered with Pucca Drain	Nil
16.6	No. of wards partly covered with Pucca Drain	17
<b>17</b>	<b>Water Supply :-</b>	
17.1	No. of Water Treatment Plant	Nil
17.2	No. of Deep Tube well	2
17.3	No. of Hand Tube well	2150
17.4	No. of Street Stand post	1093
17.5	Length of Water pipeline (in kilometer)	29.27
17.6	No. of Underground Reservoir	Nil
17.7	No. of Overhead Reservoir	1
17.8	No. of wards fully covered with water supply pipeline	Nil
17.9	No. of houses connected with Water Supply Network	Nil
17.1	Who is maintaining water supply - Municipality / PHE Dept./ KMDA / KMWSA	PHE Dept.
<b>18</b>	<b>Sewerage and Sanitation :</b>	
18.1	No. of sanitary latrine constructed	
18.2	No. of family provided with Sanitary Latrine under ILCS + BSUP / IHSDP+ HUP (together)	
18.3	No. of Community Latrine /Public Toilet	
18.4	Length of Sewer Line (in kilometer)	Nil
18.5	No. of Sewage Treatment Plant (STP)	1
<b>19</b>	<b>Solid Waste Management :</b>	
19.1	No. of Dumping Ground, if any	2
19.2	No. of Landfill site , if any	0
19.3	No of Mechanical Sweeper, if any	0
19.4	No. of Compactors, if any	0
<b>20</b>	<b>Street Light :</b>	
20.1	No. of Light Post	1944
20.2	No. of High Mast Light Post	
20.3	No. of Trident Light Post	
20.4	No. of other Ornamental Light Post	
20.5	No. of Wards covered with light posts	
<b>21</b>	<b>Health :</b>	
21.1	No. of Hospital (ULB + Govt. + Others)	3
21.2	No. of Municipal Maternity Home	1
21.3	No. of Regional Diagnostic Centre	Nil
21.4	No. of Extended Specialist Out Patient Department (ESOPD) (IPP-VIII)	Nil
21.5	No. of Municipal Health Sub-Centre	4
21.6	No. of Municipal Health Administrative Unit (HAU)( IPP-VIII)	Nil
21.7	No. of Municipal Dispensaries	0
21.8	No. of Municipal Ambulances	2
21.9	No. of Hearse Car	1
<b>22</b>	<b>Education :</b>	



22.1	No. of Higher Secondary School (Municipal)	0
22.2	No. of Higher Secondary School (others)	8
22.3	No. of Secondary School (Municipal)	0
22.4	No. of Secondary School (others)	0
22.5	No. of Primary School (Municipal)	1
22.6	No. of Primary School (others)	30
22.7	No. of Sishu Siksha Kendras (SSK)	Nil
22.8	No. of ICDS Centre	31
22.9	No. of Junior High School	Nil
22.1	No. of beneficiaries under SC/ST-scholarship	85
22.11	No. of beneficiaries under Minority scholarship	No Data Available with the municipality
<b>23</b>	<b>Other Infrastructure :</b>	
23.1	Bridge	0
23.2	Flyover	0
23.3	Stadium	0
23.4	Parks	5
23.5	Playground	8 (Private)
23.6	Auditorium/Community Hall	2
23.7	Borough Office	0
23.8	Ward office	0
23.9	ULB Market	7
23.1	Burning Ghat	3
23.11	Electric Crematorium	1
23.12	Burial Ground	7
23.13	Public Library	1
23.14	Bus Terminus	1
23.15	Ferry Ghat	6
23.16	Guest House/ Tourist Lodge	2
23.17	Road Roller	1
23.18	Cess Pool	1
23.19	No. of Slaughter House:	2
23.19.1	Municipal Slaughter House	2
23.19.2	Other Slaughter House	0
23.2	Others (Please specify)	Nil
<b>24</b>	<b>Community Structure under SJSRY :-</b>	
24.1	Total No. of CDS -	1
24.2	Total No. of NHC -	17
24.3	Total No. of NHG -	243
24.4	No. of Thrift & Credit Group (TCG)-	300
24.5	No. of SHG-	N.A
24.6	No. of DWCUA formed -	2
<b>25</b>	<b>National Social Assistance Programme (NSAP) :-</b>	

25.1	No. of beneficiaries under Indira Gandhi National Old Age Pension Scheme (IGNOAPS) -	4772
25.2	No. of beneficiaries under Indira Gandhi National Widow Pension Scheme (IGNWPS) -	1241
25.3	No. of beneficiaries under Indira Gandhi National Disability Pension Scheme (IGNDPS) -	227
25.4	No. of beneficiaries under National Family Benefit Scheme (NFBS) -	45 (for 2013-14)
26	<b>No. of Annapurna Antodaya Yojana (AY) card holder :-</b>	No Data Available with the municipality
27	<b>No. of Annapurna Anno Yojana (AAY) card holder :-</b>	No Data Available with the municipality
28	<b>No. of beneficiaries under Janani Suraksha Yojana (JSY) :-</b>	No Data Available with the municipality
4   Page30	<b>No. of beneficiaries under KANYASHREE scheme:-</b>	No Data Available with the municipality
31	<b>No. of beneficiaries under YUBASHREE scheme:-</b>	No Data Available with the municipality
32	<b>Municipal Staff( as on 01.04.2014) :-</b>	
32.1	Total No. of sanctioned Post -	232
32.2	Actual Staff Strength(Regular) -	194
32.3	Actual Staff Strength(Contractual, not Casual) -	42
33	<b>Registration of Births and Deaths during 2013-14 :-</b>	
33.1	Whether Birth & Death Certificate issued through e-governance System - Yes / No.	Yes
33.2	No. of Births Registered -	2488
33.3	No. of Birth Certificate issued -	905
33.3.1	Male	1227
33.3.2	Female	1261
33.4	No. of Death Registered -	265
33.5	No. of Death Certificate issued -	261
33.5.1	Male	154
33.5.2	Female	111
34	<b>Own Revenue (2013-14)(Rs in Lakh)</b>	
34.1	Tax Revenue	39.07
34.2	Non-Tax Revenue	88.20
34.3	Total Revenue	127.27
34.4	Percentage of collection of Own revenue to Budgeted (2013-14)Own revenue	71.99



## **Section I: Introduction**

“Housing for All” Mission for urban area will be implemented during 2015-2022 and Mission will provide central assistance to implementing agencies through States and UTs for providing houses to all eligible families/beneficiaries by 2022. Mission will be implemented as Centrally Sponsored Scheme (CSS) except for the component 1.2 of credit linked subsidy which will be implemented as a Central Sector Scheme. A beneficiary family will comprise husband, wife, unmarried sons and/or unmarried daughters. The beneficiary family should not own a pucca house either in his/her name or in the name of any member of his/her family in any part of India to be eligible to receive central assistance under the mission. States/UTs, at their discretion, may decide a cut-off date on which beneficiaries need to be resident that urban area for being eligible to take benefits under the scheme.

Mission with all its component has become effective from the date 17.06.2015 and will be implemented upto 31.03.2022. All 4041 statutory towns as per Census 2011 with focus on 500 Class I cities would be covered in three phases as follows:

- Phase I (April 2015 - March 2017) to cover 100 Cities selected from States/UTs as per their willingness.
- Phase II (April 2017 - March 2019) to cover additional 200 Cities•
- Phase III (April 2019 - March 2022) to cover all other remaining Cities•

Ministry, however, will have flexibility regarding inclusion of additional cities in earlier phases in case there is a resource backed demand from States/UTs.

The HFAPoA for Jiaganj-Azimganj has been prepared in accordance with the guidelines issued by Ministry of Housing and Urban Poverty Alleviation, Government of India. Overall approach adopted throughout the preparation of this HFAPoA has been based on four key principles,

- well rounded stakeholder consultations,
- continuous community involvement,
- providing innovative solutions and
- Coordination & validation.

Methodology adopted for preparation of HFAPoA is demonstrated in the below:

- 1) Taking Initiative for Demand Assessment Survey.
- 2) Conducting Orientation Programme with elected representative and officers of ULB.
- 3) Conducting Orientation programme with Supervisors and Enumerators.
- 4) Conducting Demand survey and complete the work.
- 5) Conducting Data Entry of the survey form and complete the work
- 6) Analysis of the data.

- 7) Filling up the requisite formats.
- 8) Planning of project with elected representatives and officers of ULB.
- 9) Preparing investment requirement and Financial plan
- 10) Finalization of HFAPoA.

The municipality has completed the mapping of all slums, field level verification and identification including existing physical infrastructure available during USHA slum survey. From the Census, 2011 data it is found that there are 62 slums with 5916 households and 26197 populations. From the USHA slum survey report it is found that there are 63 slums in the municipal area with 6311 households and 26304 populations. Therefore it is clear that over 50% of population of the municipality live in Slums. Table Number H-5 of Census 2011 regarding CENSUS HOUSES USED AS RESIDENCE AND RESIDENCE-CUM-OTHER USE BY TYPE OF STRUCTURE reveals that out of total number of census houses, 14844, there are 3164 number of houses are of semi-permanent, temporary and unclassifiable. From SECC data it is revealed that there are 2861 number of houses are of Semi-Pucka and Kutchha.

#### **IHSDP Schemes of JNNURM under JIAGANJ-AZIMGANJ MUNICIPALITY**

##### **IHSDP PHASE-I**

Project Name :

IHSDP Scheme for the Town of Jiaganj-Azimganj (Phase-I),  
Jiaganj-Azimganj, West Bengal, PH-I.

**Dwelling Unit Total - 1114      Completed : 1114 nos.**

##### **Infrastructure**

<b>1. Community Centre – no. 29</b>	<b>Completed</b>
<b>2. Livelihood center 18 no</b>	<b>Completed</b>
<b>3. Rickshaw Puller 8 Nos</b>	<b>Completed</b>
<b>4. Road –CC &amp;BT - 30830 m<sup>2</sup></b>	<b>Completed</b>
<b>5. Drain – 30830 m<sup>2</sup></b>	<b>Completed</b>
<b>6. Water supply network- 1 Unit</b>	<b>Completed</b>
<b>7. Animal Pen - 26 No.</b>	<b>Completed</b>
<b>8. Street Light - 336 nos</b>	<b>Completed</b>
<b>9. Digester – 1 Unit</b>	<b>Completed</b>

**These projects have successfully been completed.**

**Technical Assistance**

**Nodal Agency**

**MED**

**SUDA**



## **Section: 2 Salient features of HFAPoA and its linkage with proposed project and its justification**

### **2.1 General introduction on status and Prioritization for proposed project**

In summarizing the HFAPoA of Jiaganj-Azimganj Municipality, Jiaganj-Azimganj Municipality takes one vertical for implementation of the project i.e. "Beneficiary -led - construction". For this project, Jiaganj-Azimganj Municipality conducted Demand Assessment survey for getting total requirement of houses in the ULB. From this survey, the total survey in all 63 slums and all non-slums poor households. In slums 2400 number of applications received for the said scheme in 63 slums and 2094 numbers of applications were received in non slums areas of all 17 wards. 4494 houses will be constructed through "Beneficiary-led-Construction by 2022.

### **2.2. Summary of findings of HFAPoA. Physical infrastructure & Social infrastructure, Spatial, demographic and socio-economic profiles of slums/ Non slums:**

Housing for All (HFA) Scheme has been launched by the Ministry of Housing & Urban Poverty Alleviation (MoHUPA), Govt. of India in Mission mode which envisages provision of Housing for All by 2022 when the Nation completes 75 years of its Independence. The Mission seeks to address the housing requirement of urban poor including slum dwellers through following programme verticals:

- a) Redevelopment of slums with private participation
- b) Promotion of affordable Housing for weaker section through credit linked subsidy
- c) Affordable Housing in partnership with public sectors
- d) Subsidy for beneficiary-led individual house construction.

In compliance with the objective and as per direction of the Ministry of Housing & Urban Poverty Alleviation (MoHUPA) and State Urban Development agency (SUDA), West Bengal was undertake a demand survey through suitable means for accessing the actual demand of housing. For this mission Baruipur Municipality undertook Demand survey on 17.09.2015 and completed the survey on 29.09.2015. Summary of findings of survey have been given below:

## Proposed Houses in slum area

Table-3: Proposed Houses in slum area

Ward No	Name of Slum	Area of the slum in Sq.mts	Number of total Households (Including pucca)	Proposed Development Strategy i. Affordable Housing Project (AHP) ii. Credit Linked Subsidy Scheme (CLSS) iii. Beneficiary Led Construction iv. Clubbing with other Tenderable Slums
1	DHAKAPARA(S.C.-001)	10000	35	BLC-2
1	ENATULIBAGH COLONY(S.C.-060)	10000	75	BLC-2
1	ENATULIBAGH MUSLIMPARA(S.C.-061)	80000	68	BLC-2
1	ENATULIBAGH(S.C.-005)	130000	171	BLC-9
1	NUTON COLONY(S.C.-002)	510000	147	BLC-4
1	PAHARIA PARA(S.C.-004)	1730000	96	BLC-3
1	PROMOD COLONY(S.C.-003)	320000	149	BLC-5
2	MAHAJAN PATTY(S.C.-006)	10000	42	BLC-3
3	BHALLADANGA-1(S.C.-007)	40000	94	BLC-2
3	BHALLADANGA-2(S.C.-014)	40000	110	BLC-2
3	BHALLADANGA-3(S.C.-015)	40000	131	BLC-4
3	BHANGAN PARA(S.C.-011)	50000	57	BLC-2
3	CHAIPARA(S.C.-009)	80000	100	BLC-4
3	DAS PARA(S.C.-013)	80000	34	BLC-2
3	GUHAPARA(S.C.-012)	170000	107	BLC-4
3	SAMBHUNAGAR COLONY(S.C.-008)	50000	97	BLC-5
3	STATION PARA(S.C.-010)	420000	98	BLC-4
4	MORICHA PARA(S.C.-018)	40000	103	BLC-4
4	POST-OFFICE PARA(S.C.-016)	10000	55	BLC-2
4	TANTI PARA(S.C.-017)	30000	71	BLC-2
5	HARIJAN PALLY(S.C.-019)	40000	63	BLC-2
6	SIBTALLA GHAT(S.C.-020)	20000	85	BLC-3
7	BANDHPUL(S.C.-023)	30000	106	BLC-4
7	GURIPARA(S.C.-021)	30000	94	BLC-6
7	KASHIGUNJ PAKURTATAL(S.C.-026)	30000	91	BLC-5
7	KHANSPARA(S.C.-025)	30000	98	BLC-5
7	KHASPARA-2(S.C.-027)	60000	114	BLC-4
7	PULDERDHAR(S.C.-024)	50000	151	BLC-4
7	SADHAKBAG(S.C.-028)	50000	126	BLC-5
7	SAMSAN PARA(S.C.-022)	30000	69	BLC-5
8	BAHADURPUR(S.C.-030)	150000	113	BLC-5
8	BANNYASROY(S.C.-029)	250000	55	BLC-2
8	DEBIPUR SLUM(S.C.-031)	40000	115	BLC-8



8	MOHANBAGAN(S.C.-032)	110000	97	BLC-5
9	MALLIKBAGAN(S.C.-034)	30000	73	BLC-2
9	PHOOLBAGAN(S.C.-035)	60000	106	BLC-4
9	PUKURPATAL(S.C.-033)	60000	134	BLC-3
10	AMALNAGAR COLONY(S.C.-036)	10000	63	BLC-3
10	SAJINAPARA(S.C.-037)	10000	48	BLC-2
11	PULKISHOR MALPARA(S.C.-038)	80000	52	BLC-2
12	BARANAGAR-1(S.C.-042)	130000	110	BLC-2
12	BARANAGAR-2(S.C.-043)	30000	115	BLC-3
12	BARANAGAR-3(S.C.-044)	30000	82	BLC-2
12	BUROSHIBTALA(S.C.-039)	60000	90	BLC-2
12	LOHAGUNJ(S.C.-041)	40000	131	BLC-4
12	LOHAGUNJ-2(S.C.-045)	330000	105	BLC-3
12	NOYLAXMABAGAN(S.C.-040)	240000	112	BLC-3
13	BANSBAGAN(S.C.-047)	10000	130	BLC-2
13	GURIPARA(S.C.-046)	210000	163	BLC-6
13	GURIPARA-2(S.C.-063)	60000	77	BLC-3
14	BARODUARY-1(S.C.-049)	10000	53	BLC-2
14	BARODUARY-2(S.C.-050)	30000	153	BLC-8
14	BARODUARY-3(S.C.-051)	10000	115	BLC-3
14	BARODUARY-4(S.C.-052)	130000	169	BLC-9
14	NOTUN COLONY(S.C.-048)	20000	145	BLC-5
15	AMAIPARA KHASPARA(S.C.-056)	420000	120	BLC-4
15	BHALLADANGA MUSLIMPARA(S.C.-055)	20000	115	BLC-2
15	PARSHIPARA(S.C.-054)	30000	107	BLC-2
15	SUKANTAPALLY(S.C.-053)	20000	104	BLC-7
15	VELLADANGA MUSLIMPARA-2(S.C.-062)	190000	105	BLC-2
16	BABLABONA(S.C.-057)	40000	108	BLC-2
16	LEBUGAGAN(S.C.-058)	40000	137	BLC-2
17	SHIBTALLAGHAT(AZIMGANJ)(S.C.-059)	160000	72	BLC-2
<b>TOTAL</b>			<b>6311</b>	<b>BLC-2</b>

**Proposed Houses in non slum area****Table-4: Proposed Houses in non slum area**

Ward No.	Land ownership			Housing Status		Homeless, If any	Beneficiary-led Construction	Credit Linked Subsidy	Affordable Housing in Partnership	Existing Housing shortage (H+H+J)
	Own	Rented	Otherwise	Semi pucca	Kutcha					
Ward 1	145	1	4	71	79	0	120	0	30	150
Ward 2	126	9	28	77	86	0	130	10	23	163
Ward 3	79	6	23	62	46	0	85	1	22	108
Ward 4	189	18	138	308	37	0	297	14	34	345
Ward 5	140	23	6	158	11	0	115	26	28	169
Ward 6	43	10	53	97	9	0	57	12	37	106
Ward 7	0	0	0	0	0	0	0	0	0	0
Ward 8	14	0	2	12	4	0	14	0	2	16
Ward 9	133	11	6	105	45	0	131	0	19	150
Ward 10	33	1	7	37	4	0	40	0	1	41
Ward 11	15	3	10	22	6	0	20	0	8	28
Ward 12	265	22	52	141	198	0	284	2	53	339
Ward 13	224	24	27	224	51	0	253	7	15	275
Ward 14	57	3	31	39	52	0	87	0	4	91
Ward 15	144	1	11	30	126	0	143	1	12	156
Ward 16	213	3	12	110	118	0	188	14	26	228
Ward 17	74	11	62	73	74	0	130	5	12	147
<b>TOTAL</b>							<b>2094</b>	<b>92</b>	<b>326</b>	<b>2512</b>

**Land Use Pattern**

Land Use Survey to understand overall existing land use pattern within the municipality area. The picture of existing land use pattern of whole municipal area is given below.

**Table- 5: Existing Land Use Pattern of Jiaganj-Azimganj Municipal Area****Land Use Structure in comparison to UDPFI Guidelines (Small Town in Plain Area)**

Sl No.	UDPGI Guidelines		Existing		Remark
	Category	%	In ha.	%	
1.	Residential	45-50	332.45	28.52	Large portion is under agricultural land. Approximately 23% land may be utilized for residential purpose
2.	Commercial	2-3	7.87	0.67	There is a scope to develop commercial area in the municipality



Sl. No.	UDPFI Guidelines		Existing		Remark
	Category	%	In ha.	%	
3.	Industrial	8-10	1.09	0.09	Non-polluting industry may be developed in the municipality
4.	Public & Semi Public	6-8	36.97	3.17	There are number of plans to develop public & semi-public infrastructure in the municipality
5.	Recreational	12-14	194.00	16.63	Open spaces in the developed area may be utilized for recreational purposes.
6.	Transport	10-12	46.07	3.95	New road network may be developed in the fringe area.
7.	Agricultural & Water Bodies	Balance	547.55	46.95	Water bodies should be conserved and protected for various purposes.
	Total	100	1166.00	100	

Status of all slums and non slum in respect of the infrastructures is detailed below:

In this section of DPR, analysis was made on the existing situation of intra-municipal infrastructure, slum infrastructure, trans-municipal infrastructure, and land-use and environment improvement and after identifying the need, the priority has been assigned and accordingly planning was done.

#### • *Water Supply*

The main source of drinking water in the municipality is ground water through tube well, bore wells and PHE's deep bore well supply water in some areas. Two separate projects for supplying pure drinking water through pipe line is under development.

#### • *Sanitation*

At present, there is no sewage treatment plant working in the municipality. The open drains in these areas carry the sewage as well as the storm water runoff of the area.

#### • *Solid Waste Management*

There is door to door two bin collection system in the entire municipal area including the slums; however, the collection system is not satisfactory in the fringe areas. Under the present infrastructure the entire municipal area could not be covered.

#### • *Local drainage*

The present Drainage condition and water logging during the monsoon season is a major problem in the municipality as well as in the slum areas. Most of these 53 slums do not have proper drainage system.

• **Access Roads, with adequate street lighting**

The road networks in these slums are not satisfactory, 30% of the roads in the slums are kutchha and they become almost inaccessible in the monsoon. The main roads connecting the slums in the fringe area are either kutchha or morum or in bad condition. These need major repairing, maintenance and extension.

All slum areas have access to electricity connections. Most of the roads in slum areas have street light facilities.

• **Social Infrastructure (e.g. for health, community spaces)**

Different Social Infrastructures like Health centres, Schools, Bus Stands etc. are in close vicinity in respect of all slums.

## Project Justification

For the following reasons Jiaganj-Azimganj Municipality selected the slums and non-slums namely mentioned below as first project for preparation of DPR under HFAPoA (PMAY):

**Table-6: Justification of the Project**

Sl.No	Name of the Slums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
1	DHAKAPARA(S.C.-001)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
2	NUTON COLONY(S.C.-002)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
3	PROMOD COLONY(S.C.-003)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
4	PAHARIA PARA(S.C.-004)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space



SLNo	Name of the Slums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
5	ENATULIBAGH(S.C.-005)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
6	ENATULIBAGH COLONY(S.C.-060)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
7	ENATULIBAGH MUSLIMPARA(S.C.-061)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
8	MAHAJAN PATTY(S.C.-006)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
9	BHALLADANGA-1(S.C.-007)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
10	SAMBHUNAGAR COLONY(S.C.-008)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
11	CHAI PARA(S.C.-009)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
12	STATION PARA(S.C.-010)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
13	BHANGAN PARA(S.C.-011)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
14	GUHAPARA(S.C.-012)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
15	DAS PARA(S.C.-013)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
16	BHALLADANGA-2(S.C.-014)	The condition of living in the slum is unhygienic	Own Land	37	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space



SLNo	Name of the Shums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
17	BHALLADANGA-3(S.C.-015)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
18	POST-OFFICE PARA(S.C.-016)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
19	TANTI PARA(S.C.-017)	The condition of living in the slum is unhygienic	Own Land	41	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
20	MORICHA PARA(S.C.-018)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
21	SIBTALLA GHAT(S.C.-020)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
22	HARIJAN PALLY(S.C.-019)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
23	GURIPARA(S.C.-021)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
24	SAMSAN PARA(S.C.-022)	The condition of living in the slum is unhygienic	Own Land	37	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
25	BANDHPUL(S.C.-023)	The condition of living in the slum is unhygienic	Own Land	37	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
26	PULDERDHAR(S.C.-024)	The condition of living in the slum is unhygienic	Own Land	36	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
27	KHANSPARA(S.C.-025)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
28	KASHIGUNJ PAKURTATAL(S.C.-026)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space



SLNo	Name of the Slums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
29	KHASPARA-2(S.C.-027)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
30	SADHAKBAG(S.C.-028)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
31	BANNYASROY(S.C.-029)	The condition of living in the slum is unhygienic	Own Land	37	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
32	BAHADURPUR(S.C.-030)	The condition of living in the slum is unhygienic	Own Land	42	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
33	DEBIPUR SLUM(S.C.-031)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
34	MOHANBAGAN(S.C.-032)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
35	PUKURPATAL(S.C.-033)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
36	MALLIKBAGAN(S.C.-034)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
37	PHOOLBAGAN(S.C.-035)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
38	AMALNAGAR COLONY(S.C.-036)	The condition of living in the slum is unhygienic	Own Land	33	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
39	SAJINAPARA(S.C.-037)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
40	PULKISHOR MALPARA(S.C.-038)	The condition of living in the slum is unhygienic	Own Land	39	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space



SLNo	Name of the Slums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
41	BUROSHIBTALA(S.C.-039)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
42	NOYLAXMABAGA N(S.C.-040)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
43	LOHAGUNJ(S.C.-041)	The condition of living in the slum is unhygienic	Own Land	42	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
44	BARANAGAR-1(S.C.-042)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
45	BARANAGAR-2(S.C.-043)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
46	BARANAGAR-3(S.C.-044)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
47	LOHAGUNJ-2(S.C.-045)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
48	GURIPARA(S.C.-046)	The condition of living in the slum is unhygienic	Own Land	31	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
49	BANSBAGAN(S.C.-047)	The condition of living in the slum is unhygienic	Own Land	33	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
50	NOTUN COLONY(S.C.-048)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
51	BARODUARY-1(S.C.-049)	The condition of living in the slum is unhygienic	Own Land	39	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
52	BARODUARY-2(S.C.-050)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space



SLNo	Name of the Slums	Status	Land	Age in years	National High Way	Status of Housings	Road Status	Habitation pattern
53	BARODUARY-3(S.C.-051)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
54	BARODUARY-4(S.C.-052)	The condition of living in the slum is unhygienic	Own Land	42	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
55	SUKANTAPALLY(S.C.-053)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
56	PARSHIPARA(S.C.-054)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
57	BHALLADANGA MUSLIMPARA(S.C.-055)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
58	AMAIPARA KHASPARA(S.C.-056)	The condition of living in the slum is unhygienic	Own Land	32	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
59	BABLABONA(S.C.-057)	The condition of living in the slum is unhygienic	Own Land	35	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
60	LEBUGAGAN(S.C.-058)	The condition of living in the slum is unhygienic	Own Land	37	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
61	SHIBTALLAGHAT(AZIMGANJ)(S.C.-059)	The condition of living in the slum is unhygienic	Own Land	42	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
62	VELLADANGA MUSLIMPARA-2(S.C.-062)	The condition of living in the slum is unhygienic	Own Land	34	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space
63	GURIPARA-2(S.C.-063)	The condition of living in the slum is unhygienic	Own Land	38	The National Highway - 34 is 20.0 kms away	Major population is living in huts, made of darma / bricks with tin sheets and asbestos/tiles on roof	Majority portion of roads are brick paved or damaged roads.	Habitation pattern in the slums is congested with insufficient open space

### 2.3 Tenure Status

As per the demand survey and geographical location of the city out of four verticals municipality has taken only Beneficiary Lead Construction (BLC) for the year 2016-17. In the 1st year of implementation of

Housing for All, 1477 beneficiaries have been identified for the construction of New House through BLC. The above beneficiaries have been selected only who have their own land required for the construction of new house under BLC.

## 2.4 Choice of Option/Vertical and its justification for housing and/or infrastructure

- “In-situ” Slum Redevelopment using land as Resource( include viability analysis)
- Credit-Linked Subsidy Scheme (CLSS)
- Affordable Housing in Partnership (AHP)
- Beneficiary-led individual house construction or enhancement

In the case of Jiaganj-Azimganj Municipality, Municipality takes only one vertical i.e. is “ Beneficiary led construction”. From present Demand Assessment survey for Housing for all (HFA), it is noticed that 4494 household covering under this project. 4494 houses will be constructed through “Beneficiary-led-Construction” Under “Beneficiary-led-Construction”, and each beneficiary will get 1.5 lakh from central assistance.

Table-7: Slum-wise Intervention strategies for Tenable Slums

Ward No	Name of Slum	Area of the slum in Sq.mts	Number of total Households (Including pucca)	Proposed Development Strategy
				i. Affordable Housing Project (AHP)
				ii. Credit Linked Subsidy Scheme (CLSS)
				iii. Beneficiary Led Construction
				iv. Clubbing with other Tenderable Slums
1	DHAKAPARA(S.C.-001)	10000	35	BLC-2
1	ENATULIBAGH COLONY(S.C.-060)	10000	75	BLC-2
1	ENATULIBAGH MUSLIMPARA(S.C.-061)	80000	68	BLC-2
1	ENATULIBAGH(S.C.-005)	130000	171	BLC-9
1	NUTON COLONY(S.C.-002)	510000	147	BLC-4
1	PAHARIA PARA(S.C.-004)	1730000	96	BLC-3
1	PROMOD COLONY(S.C.-003)	320000	149	BLC-5
2	MAHAJAN PATTY(S.C.-006)	10000	42	BLC-3
3	BHALLADANGA-1(S.C.-007)	40000	94	BLC-2
3	BHALLADANGA-2(S.C.-014)	40000	110	BLC-2
3	BHALLADANGA-3(S.C.-015)	40000	131	BLC-4
3	BHANGAN PARA(S.C.-011)	50000	57	BLC-2



3	CHAIPARA(S.C.-009)	80000	100	BLC-4
3	DAS PARA(S.C.-013)	80000	34	BLC-2
3	GUHAPARA(S.C.-012)	170000	107	BLC-4
3	SAMBHUNAGAR COLONY(S.C.-008)	50000	97	BLC-5
3	STATION PARA(S.C.-010)	420000	98	BLC-4
4	MORICHA PARA(S.C.-018)	40000	103	BLC-4
4	POST-OFFICE PARA(S.C.-016)	10000	55	BLC-2
4	TANTI PARA(S.C.-017)	30000	71	BLC-2
5	HARIJAN PALLY(S.C.-019)	40000	63	BLC-2
6	SIBTALLA GHAT(S.C.-020)	20000	85	BLC-3
7	BANDHPUL(S.C.-023)	30000	106	BLC-4
7	GURIPARA(S.C.-021)	30000	94	BLC-6
7	KASHIGUNJ PAKURTATAL(S.C.-026)	30000	91	BLC-5
7	KHANSPARA(S.C.-025)	30000	98	BLC-5
7	KHASPARA-2(S.C.-027)	60000	114	BLC-4
7	PULDERDHAR(S.C.-024)	50000	151	BLC-4
7	SADHAKBAG(S.C.-028)	50000	126	BLC-5
7	SAMSAN PARA(S.C.-022)	30000	69	BLC-5
8	BAHADURPUR(S.C.-030)	150000	113	BLC-5
8	BANNYASROY(S.C.-029)	250000	55	BLC-2
8	DEBIPUR SLUM(S.C.-031)	40000	115	BLC-8
8	MOHANBAGAN(S.C.-032)	110000	97	BLC-5
9	MALLIKBAGAN(S.C.-034)	30000	73	BLC-2
9	PHOOLBAGAN(S.C.-035)	60000	106	BLC-4
9	PUKURPATAL(S.C.-033)	60000	134	BLC-3
10	AMALNAGAR COLONY(S.C.-036)	10000	63	BLC-3
10	SAJINAPARA(S.C.-037)	10000	48	BLC-2
11	PULKISHOR MALPARA(S.C.-038)	80000	52	BLC-2
12	BARANAGAR-1(S.C.-042)	130000	110	BLC-2
12	BARANAGAR-2(S.C.-043)	30000	115	BLC-3
12	BARANAGAR-3(S.C.-044)	30000	82	BLC-2
12	BUROSHIBTALA(S.C.-039)	60000	90	BLC-2
12	LOHAGUNJ(S.C.-041)	40000	131	BLC-4
12	LOHAGUNJ-2(S.C.-045)	330000	105	BLC-3
12	NOYLAXMABAGAN(S.C.-040)	240000	112	BLC-3
13	BANSBAGAN(S.C.-047)	10000	130	BLC-2

13	GURIPARA(S.C.-046)	210000	163	BLC-6
13	GURIPARA-2(S.C.-063)	60000	77	BLC-3
14	BARODUARY-1(S.C.-049)	10000	53	BLC-2
14	BARODUARY-2(S.C.-050)	30000	153	BLC-8
14	BARODUARY-3(S.C.-051)	10000	115	BLC-3
14	BARODUARY-4(S.C.-052)	130000	169	BLC-9
14	NOTUN COLONY(S.C.-048)	20000	145	BLC-5
15	AMAIPARA KHASPARA(S.C.-056)	420000	120	BLC-4
15	BHALLADANGA MUSLIMPARA(S.C.-055)	20000	115	BLC-2
15	PARSHIPARA(S.C.-054)	30000	107	BLC-2
15	SUKANTAPALLY(S.C.-053)	20000	104	BLC-7
15	VELLADANGA MUSLIMPARA-2(S.C.-062)	190000	105	BLC-2
16	BABLABONA(S.C.-057)	40000	108	BLC-2
16	LEBUGAGAN(S.C.-058)	40000	137	BLC-2
17	SHIBTALLAGHAT(AZIMGANJ)(S.C.-059)	160000	72	BLC-2

Table-8: Year-wise Proposed Interventions for Other Urban Poor based on demand survey

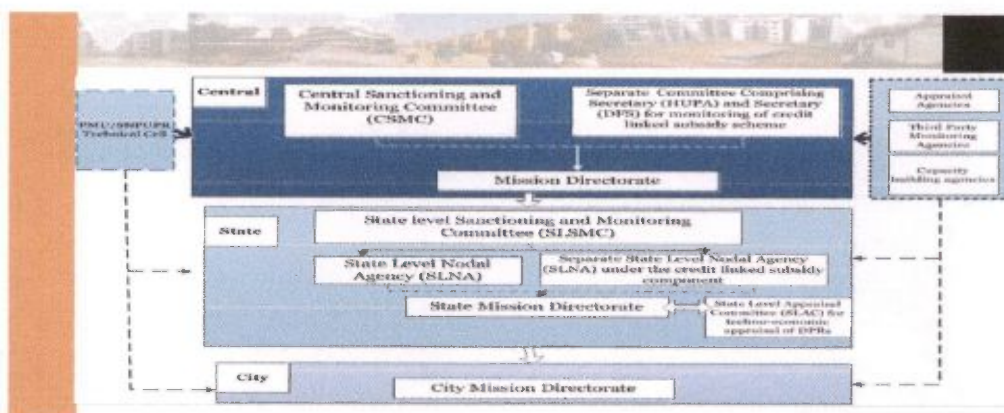
Year	Number of Beneficiaries and Central Assistance Required (Rs. in Crores)									
	Beneficiary-led Construction		Credit Linked Subsidy		Affordable Housing in Partnership		Future Projected Urban Poor (Through AHP)		Total	
	No. of Beneficiaries	Amount	No. of Beneficiaries	Amount	No. of Beneficiaries	Amount	No. of Beneficiaries	Amount	No. of Beneficiaries	Amount
2015-16	0	0.00	0	0.00	0	0.00	0	0.00	0	0.00
2016-17	600	9.00	45	0.00	0	0.00	0	0.00	645	9.00
2017-18	350	5.25	10	0.00	166	2.49	749	11.24	1275	18.98
2018-19	540	8.10	10	0.00	70	1.05	414	6.21	1034	15.36
2019-20	350	5.25	10	0.00	40	0.60	443	6.65	843	12.50
2020-21	131	1.97	9	0.00	30	0.45	474	7.11	644	9.53
2021-22	123	1.85	8	0.00	20	0.30	507	7.61	658	9.75
<b>Total</b>	<b>2094</b>	<b>31.41</b>	<b>92</b>	<b>0.00</b>	<b>326</b>	<b>4.89</b>	<b>2587</b>	<b>38.81</b>	<b>5099</b>	<b>75.11</b>

## 2.5 Resource mobilization strategy and Implementation strategy

Physical and social infrastructure require to develop in slum and non slum area to be covered another central and state schemes like 13th FC, 4th SFC, UWES etc. Beneficiaries belong to pro poor families, unable to contribute the beneficiary contribution under HFA project should be cover under project of SUHP funded by State Government.



**Figure-4: Resource mobilization strategy and Implementation strategy**



### **Roles and responsibilities of the Institutions:**

#### **Central Sanctioning and Monitoring Committee (CSMC)**

- An inter-ministerial committee under Chairpersonship of Secretary (HUPA) for implementation of the Mission, approvals there under and monitoring.

#### **Indicative Functions of CSMC**

- Overall review and Monitoring of the Mission
- Assessing resource requirement based on HFAPoA and AIP submitted by States/UTs
- Approval of central releases under various components of the Mission
- Approval of Capacity Building Plans of States/UTs
- Devising financial and other norms for various activities undertaken as part of the Mission
- Approval of Annual Quality Monitoring Plans, Social Audit plans etc.
- Any other important issues required for implementation of the Mission.

#### **State Level Sanctioning and Monitoring Committee (SLSMC)**

##### **Indicative functions of SLSMC**

- Approval of Housing for All Plan of Action (HFAPoA)
- Approval of Annual Implementation Plan
- Approval of DPRs under various components of the Mission
- Approval of Annual Quality Monitoring Plans
- Reviewing progress of approved projects in the State and cities
- Monitoring of implementation of Mission
- Any other issues required for effective implementation of the Mission.

## **Section 3: Project Concept and Scope**

### **3.1 Introduction of slum(s)/non Slum Area**

Under section-3 of the Slum Area Improvement and Clearance Act, 1956, slums have been defined as mainly those residential areas where dwellings are in any respect unfit for human habitation by reasons of dilapidation, overcrowding, faulty arrangements and designs of such buildings, narrowness and faulty arrangement of streets, lack ventilation, light or sanitation facilities or any combination of these factors which are detrimental to safety, health and morals. Thus, conceptually slums are compact overcrowded residential areas (and not isolated or scattered dwellings) unfit for habitation due to lack of one or more of the basic infrastructure like drinking water, sanitation, electricity, sewerage, streets etc.

It is in this background that in the 2001 Census, an innovative attempt was made to collect demographic data slum areas across the country.

As per 2001 population census, the slum population is estimated to be 61.8 million, out of a total urban population of 285.35 million people reside in urban areas.

The analysis of the data in this report provided an overview of the population characteristics of slums and squatter settlements and is expected to serve as a benchmark for pragmatic and realistic town planning while dealing with the issue of slums and slum dwellers.

Urbanization is fast becoming the defining process in shaping the course of social transformation & ensuing development concerns in India. About 377 million persons or about 31% of India's population of 1.21 billion lived in urban areas in 2011, spread over 5161 towns.

As per Report on Indian Urban Infrastructure and Services (NIUA) Report, the urban population is likely to grow to about 600 million by 2031. About one-fourth (24%) of the urban population of India is



poor i.e. their expenditure on consumption goods is less than the poverty line benchmark. The benefits of urbanization have eluded this burgeoning 67 million urban poor population, most of who live in slums. An analysis of population growth trends between 1991 and 2001 shows that while India grew at an average annual growth rate of 2%, urban India grew at 3% mega cities at 4% and slum populations rose by 5%. This rapid and unplanned urbanization and simultaneous growth of urban population in the limited living spaces has a visible impact on the quality of life of the slum dwellers of the city.

It is increasing clear that sustainable growth can only take place when it is inclusive and when the entire population including the poor and marginalized need to have at the least access to decent shelter, basic amenities, livelihoods and a voice in governance. Keeping this in mind the Government of India and the various State Governments have been taking up several schemes on partnership mode.

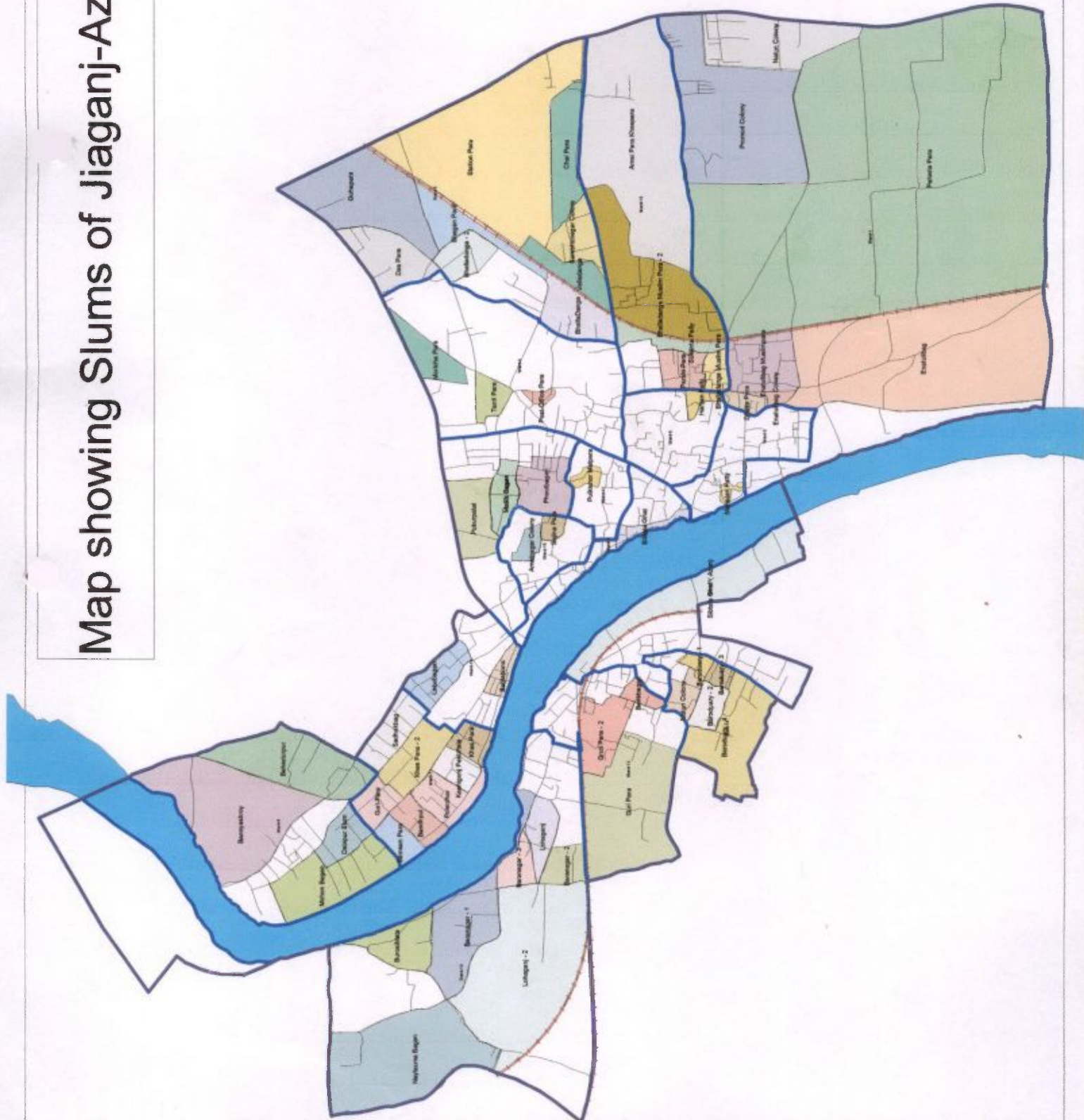
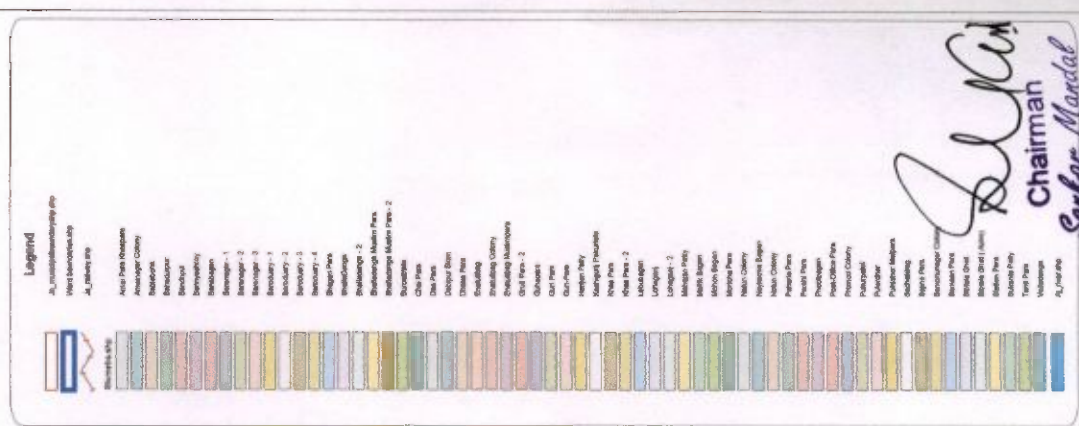
**Table-9: Introduction of slum(s)/non Slum Area**

Sl. No	Ward no	Slum Code	Slum Name	AREA in Sq Mt
1	1	1	DHAKAPARA(S.C.-001)	10000
2	1	2	NUTON COLONY(S.C.-002)	510000
3	1	3	PROMOD COLONY(S.C.-003)	320000
4	1	4	PAHARIA PARA(S.C.-004)	1730000
5	1	5	ENATULIBAGH(S.C.-005)	130000
6	2	6	MAHAJAN PATTY(S.C.-006)	10000
7	3	7	BHALLADANGA-1(S.C.-007)	40000
8	3	8	SAMBHUNAGAR COLONY(S.C.-008)	50000
9	3	9	CHAIPARA(S.C.-009)	80000
10	3	10	STATION PARA(S.C.-010)	420000
11	3	11	BHANGAN PARA(S.C.-011)	50000
12	3	12	GUHAPARA(S.C.-012)	170000
13	3	13	DAS PARA(S.C.-013)	80000
14	3	14	BHALLADANGA-2(S.C.-014)	40000
15	3	15	BHALLADANGA-3(S.C.-015)	40000
16	4	16	POST-OFFICE PARA(S.C.-016)	10000
17	4	17	TANTI PARA(S.C.-017)	30000
18	4	18	MORICHA PARA(S.C.-018)	40000
19	5	19	HARIJAN PALLY(S.C.-019)	40000
20	6	20	SIBTALLA GHAT(S.C.-020)	20000
21	7	21	GURIPARA(S.C.-021)	30000
22	7	22	SAMSAN PARA(S.C.-022)	30000
23	7	23	BANDHPUL(S.C.-023)	30000
24	7	24	PULDERDHAR(S.C.-024)	50000
25	7	25	KHANSPARA(S.C.-025)	30000
26	7	26	KASHIGUNJ PAKURTATAL(S.C.-026)	30000
27	7	27	KHASPARA-2(S.C.-027)	60000
28	7	28	SADHAKBAG(S.C.-028)	50000

Sl. No	Ward no	Slum Code	Slum Name	AREA in Sq Mt
29	8	29	BANNYASROY(S.C.-029)	250000
30	8	30	BAHADURPUR(S.C.-030)	150000
31	8	31	DEBIPUR SLUM(S.C.-031)	40000
32	8	32	MOHANBAGAN(S.C.-032)	110000
33	9	33	PUKURPATAL(S.C.-033)	60000
34	9	34	MALLIKBAGAN(S.C.-034)	30000
35	9	35	PHOOLBAGAN(S.C.-035)	60000
36	10	36	AMALNAGAR COLONY(S.C.-036)	10000
37	10	37	SAJINAPARA(S.C.-037)	10000
38	11	38	PULKISHOR MALPARA(S.C.-038)	80000
39	12	39	BUROSHIBTALA(S.C.-039)	60000
40	12	40	NOYLAXMABAGAN(S.C.-040)	240000
41	12	41	LOHAGUNJ(S.C.-041)	40000
42	12	42	BARANAGAR-1(S.C.-042)	130000
43	12	43	BARANAGAR-2(S.C.-043)	30000
44	12	44	BARANAGAR-3(S.C.-044)	30000
45	12	45	LOHAGUNJ-2(S.C.-045)	330000
46	13	46	GURIPARA(S.C.-046)	210000
47	13	47	BANSBAGAN(S.C.-047)	10000
48	14	48	NOTUN COLONY(S.C.-048)	20000
49	14	49	BARODUARY-1(S.C.-049)	10000
50	14	50	BARODUARY-2(S.C.-050)	30000
51	14	51	BARODUARY-3(S.C.-051)	10000
52	14	52	BARODUARY-4(S.C.-052)	130000
53	15	53	SUKANTAPALLY(S.C.-053)	20000
54	15	54	PARSHIPARA(S.C.-054)	30000
55	15	55	BHALLADANGA MUSLIMPARA(S.C.-055)	20000
56	15	56	AMAIPARA KHASPARA(S.C.-056)	420000
57	16	57	BABLABONA(S.C.-057)	40000
58	16	58	LEBUGAGAN(S.C.-058)	40000
59	17	59	SHIBTALLAGHAT(AZIMGANJ)(S.C.-059)	160000
60	1	60	ENATULIBAGH COLONY(S.C.-060)	10000
61	1	61	ENATULIBAGH MUSLIMPARA(S.C.-061)	80000
62	15	62	VELLADANGA MUSLIMPARA-2(S.C.-062)	190000
63	13	63	GURIPARA-2(S.C.-063)	60000



# Map showing Slums of Jiaganj-Azimganj Municipality



*Sarbar Mandal*  
**Chairman**  
 Jiaganj-Azimganj Municipality

## Non Slum Area

Table-10: Non Slum Area

Ward No	Area in Sqkm
Ward-1	3.11
Ward-2	0.29
Ward-3	1.14
Ward-4	0.61
Ward-5	0.32
Ward-6	0.1
Ward-8	0.98
Ward-9	0.38
Ward-10	0.19
Ward-11	0.17
Ward-12	1.32
Ward-13	0.53
Ward-14	0.39
Ward-15	1.03
Ward-16	0.31
Ward-17	0.46
<b>Total</b>	<b>11.66</b>





**3.2. Location of slum(s) / non Slum Area, Tenure Status, Land use and Land Possession status****Table-11: Location of slum(s) / non Slum Area, Tenure Status, Land use and Land Possession status**

Slum Name	Slum Location	Age of Slum	Ownership of Land	Tenability (Yes/no)	Land Value (Z1 is high and Z4 is low)
DHAKAPARA(S.C.-001)	Fringe area	36	Own Land	Yes	Z4
ENATULIBAGH COLONY(S.C.-060)	Fringe area	31	Own Land	Yes	Z4
ENATULIBAGH MUSLIMPARA(S.C.-061)	Core Area	36	Own Land	Yes	Z4
ENATULIBAGH(S.C.-005)	Fringe area	32	Own Land	Yes	Z4
NUTON COLONY(S.C.-002)	Core Area	31	Own Land	Yes	Z4
PAHARIA PARA(S.C.-004)	Fringe area	35	Own Land	Yes	Z4
PROMOD COLONY(S.C.-003)	Fringe area	38	Own Land	Yes	Z3
MAHAJAN PATTY(S.C.-006)	Fringe area	35	Own Land	Yes	Z1
BHALLADANGA-1(S.C.-007)	Fringe area	36	Own Land	Yes	Z3
BHALLADANGA-2(S.C.-014)	Fringe area	31	Own Land	Yes	Z3
BHALLADANGA-3(S.C.-015)	Fringe area	31	Own Land	Yes	Z3
BHANGAN PARA(S.C.-011)	Fringe area	32	Own Land	Yes	Z3
CHAIPARA(S.C.-009)	Fringe area	31	Own Land	Yes	Z3
DAS PARA(S.C.-013)	Fringe area	36	Own Land	Yes	Z3
GUHAPARA(S.C.-012)	Core Area	35	Own Land	Yes	Z2
SAMBHUNAGAR COLONY(S.C.-008)	Fringe area	37	Own Land	Yes	Z2
STATION PARA(S.C.-010)	Core Area	36	Own Land	Yes	Z2
MORICHA PARA(S.C.-018)	Core Area	32	Own Land	Yes	Z4
POST-OFFICE PARA(S.C.-016)	Fringe area	41	Own Land	Yes	Z1
TANTI PARA(S.C.-017)	Core Area	32	Own Land	Yes	Z2
HARIJAN PALLY(S.C.-019)	Core Area	36	Own Land	Yes	Z4
SIBTALLA GHAT(S.C.-020)	Fringe area	31	Own Land	Yes	Z1
BANDHPUL(S.C.-023)	Fringe area	35	Own Land	Yes	Z4



Slum Name	Slum Location	Age of Slum	Ownership of Land	Tenability (Yes/no)	Land Value (Z1 is high and Z4 is low)
GURIPARA(S.C.-021)	Fringe area	37	Own Land	Yes	Z4
KASHIGUNJ PAKURTATAL(S.C.-026)	Fringe area	37	Own Land	Yes	Z4
KHANSPARA(S.C.-025)	Fringe area	36	Own Land	Yes	Z4
KHASPARA-2(S.C.-027)	Fringe area	38	Own Land	Yes	Z4
PULDERDHAR(S.C.-024)	Fringe area	35	Own Land	Yes	Z4
SADHAKBAG(S.C.-028)	Fringe area	32	Own Land	Yes	Z4
SAMSAN PARA(S.C.-022)	Fringe area	35	Own Land	Yes	Z4
BAHADURPUR(S.C.-030)	Fringe area	37	Own Land	Yes	Z1
BANNYASROY(S.C.-029)	Fringe area	42	Own Land	Yes	Z4
DEBIPUR SLUM(S.C.-031)	Fringe area	35	Own Land	Yes	Z4
MOHANBAGAN(S.C.-032)	Core Area	38	Own Land	Yes	Z4
MALLIKBAGAN(S.C.-034)	Core Area	34	Own Land	Yes	Z1
PHOOLBAGAN(S.C.-035)	Core Area	34	Own Land	Yes	Z1
PUKURPATAL(S.C.-033)	Core Area	31	Own Land	Yes	Z2
AMALNAGAR COLONY(S.C.-036)	Core Area	33	Own Land	Yes	Z1
SAJINAPARA(S.C.-037)	Core Area	32	Own Land	Yes	Z1
PULKISHOR MALPARA(S.C.-038)	Core Area	39	Own Land	Yes	Z1
BARANAGAR-1(S.C.-042)	Core Area	35	Own Land	Yes	Z2
BARANAGAR-2(S.C.-043)	Core Area	38	Own Land	Yes	Z2
BARANAGAR-3(S.C.-044)	Fringe area	42	Own Land	Yes	Z2
BUROSHIBTALA(S.C.-039)	Fringe area	35	Own Land	Yes	Z3
LOHAGUNJ(S.C.-041)	Fringe area	38	Own Land	Yes	Z3
LOHAGUNJ-2(S.C.-045)	Fringe area	34	Own Land	Yes	Z3
NOYLAXMABAGAN(S.C.-040)	Fringe area	34	Own Land	Yes	Z1
BANSBAGAN(S.C.-047)	Fringe area	31	Own Land	Yes	Z4
GURIPARA(S.C.-046)	Fringe area	33	Own Land	Yes	Z3
GURIPARA-2(S.C.-063)	Fringe area	32	Own Land	Yes	Z3

Slum Name	Slum Location	Age of Slum	Ownership of Land	Tenability (Yes/no)	Land Value (Z1 is high and Z4 is low)
BARODUARY-1(S.C.-049)	Core Area	39	Own Land	Yes	Z1
BARODUARY-2(S.C.-050)	Core Area	35	Own Land	Yes	Z1
BARODUARY-3(S.C.-051)	Fringe area	38	Own Land	Yes	Z1
BARODUARY-4(S.C.-052)	Fringe area	42	Own Land	Yes	Z1
NOTUN COLONY(S.C.-048)	Fringe area	35	Own Land	Yes	Z2
AMAI PARA KHASPARA(S.C.-056)	Fringe area	38	Own Land	Yes	Z4
HALLADANGA MUSLIMPARA(S.C.-055)	Fringe area	34	Own Land	Yes	Z3
PARSHIPARA(S.C.-054)	Core Area	32	Own Land	Yes	Z4
SUKANTAPALLY(S.C.-053)	Fringe area	35	Own Land	Yes	Z3
VELLADANGA MUSLIMPARA-2(S.C.-062)	Fringe area	37	Own Land	Yes	Z4
BABLABONA(S.C.-057)	Fringe area	42	Own Land	Yes	Z1
LEBUGAGAN(S.C.-058)	Core Area	34	Own Land	Yes	Z1
SHIBTALLAGHAT(AZIMGANJ)(S.C.-059)	Core Area	38	Own Land	Yes	Z1



### 3.3 Existing basic infrastructure and its coverage

#### The project slums and existing scenario of infrastructure:

63 nos Slums and 17 non slums have been selected as a First Project under PMAY scheme by Jiaganj-Azimganj Municipality in consultation with the state level Nodal Agency - The State Urban Development Agency (SUDA) under M.A. Department, GoWB.

**Table-12: The project slums and existing scenario of infrastructure**

Slum Name	Anganwadi under ICDS	No. of Aganwadis	No. of Municipal Pre-school	No. of Private Pre-school	No. of Municipal Primary-school	State Government school	No. of State Government schools	Private High school	No. of Private High schools	Adult Education Centre	Non-formal Education Centre	Urban Health Post	Primary Health Centre	Government Hospital	Maternity Centre	Private Clinic	Registered Medical Practitioner(RMP)	Ayurvedic Doctor/Valdya	Community Hall	Livelihood/Production Centre	Vocational training/Training-cum-production centre	Street Children Rehabilitation Centre	Night Shelter	Old Age home	Old Age Pension (No. of holders)	Widow Pension (No. of holders)	Disabled Pension (No. of holders)	General Insurance (No. Covered)	Health Insurance (No. Covered)	Self Help Groups/DWCUA Groups in Slum	Thrift and Credit Societies in Slum	Slum-dwellers Association
DHAKA PARA(S.C.-001)	Within the slum	1	0	0	0	0.5 to 1.0 kms	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
NUTON COLON Y(S.C.-002)	Data not Entered	0	0	0	0	Data not Entered	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PROMO D COLON Y(S.C.-003)	Within the slum	1	0	0	0	With in the slum	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0





[illegible]

[illegible]



[illegible]

[illegible]



[illegible]

[illegible]



[illegible]

[illegible]





AGAN(S .C.-058)	the slum			hin the slum	to 2.0 km s	ret han 5.0 km s	to 2.0 kms	ret han 5.0 km s	ret han 5.0 km s	ret han 5.0 km s	or eth an 5.0 km s	ret han 5.0 km s	ret han 5.0 km s	ret han 5.0 km s	to 2.0 km s	ret han 5.0 km s	ret han 5.0 km s	5	2	7
SHIBTA LLAGH AT(AZI MGANJ) (S.C.- 059)	1.0 to 2.0 kms	0	0	0	0	0.5 to 1.0 kms	1.0 to 2.0 km s	1.0 to 2.0 km s	0	Mo ret han 5.0 km s	2.0 to 5.0 km s	2.0 to 5.0 km s	2.0 to 5.0 km s	2.0 to 5.0 km s	Mo ret han 5.0 km s	Mo ret han 5.0 km s	Mo ret han 5.0 km s	1	5	1
																		0	5	2

## Social Infrastructure at a glance

Table-13: Social Infrastructure at a glance

SL No	Name of the Slums	The project slum site	War d No	Road Type Running In front of the Slum	slum connects it to major areas	Distance of National High Way	Slum Age	Area in sqm	Owne rship of slum	Exist ing Hous e Hold	Popul ation	Slum Dwellers' Occupation	Envi ron ment al Con ditio n	Condition of Drain	Road Conditi on	Street Light	SW status	Housi ng Cond ition	Water Supply
1	DHAKAPARA(S .C.-001)	Fringe area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	35	164	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt



2	ENATULIBAGH COLONY(S.C.-060)	Fringe area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	51000	The owner ship of land lies with Own	75	300	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
3	ENATULIBAGH MUSLIMPARA(S.C.-061)	Core Area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	32000	The owner ship of land lies with Own	68	329	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
4	ENATULIBAGH (S.C.-005)	Fringe area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	17300	The owner ship of land lies with Own	171	711	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
5	NUTON COLONY(S.C.-002)	Core Area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	13000	The owner ship of land lies with Own	147	654	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
6	PAHARIA PARA(S.C.-004)	Fringe area	1	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	96	388	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient

7	PROMOD COLONY(S.C.-003)	Fringe area	1	Metal road is running in front of the slums	Azimganj Municipality	away	More than 20 years	80000	The owner ship of land lies with Own	149	598	local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The slum is poor	The slum is partially covered with surface drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
8	MAHAJAN PATTY(S.C.-006)	Fringe area	2	Metal road is running in front of the slums	Azimganj Municipality	away	More than 20 years	10000	The owner ship of land lies with Own	42	158	local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The slum is poor	The slum is partially covered with surface drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
9	BHALLADANG A-1(S.C.-007)	Fringe area	3	Metal road is running in front of the slums	Azimganj Municipality	away	More than 20 years	40000	The owner ship of land lies with Own	115	475	local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The slum is poor	The slum is partially covered with surface drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
10	BHALLADANG A-2(S.C.-014)	Fringe area	3	Metal road is running in front of the slums	Azimganj Municipality	away	More than 20 years	50000	The owner ship of land lies with Own	110	461	local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The slum is poor	The slum is partially covered with surface drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient



11	BHALLADANG A-3(S.C.-015)	Fringe area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	80000	The owner ship of land lies with Own	131	549	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kaach or dilapi dated	Water supply is sufficient
12	BHANGAN PARA(S.C.-011)	Fringe area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	42000	The owner ship of land lies with Own	57	226	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kaach or dilapi dated	Water supply is sufficient
13	CHAIPARA(S.C.-009)	Fringe area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	50000	The owner ship of land lies with Own	100	422	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kaach or dilapi dated	Water supply is sufficient

14	DAS PARA(S.C.-013)	Fringe area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	17000	The owner ship of land lies with Own	34	153	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
15	GUHAPARA(S. C.-012)	Core Area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	80000	The owner ship of land lies with Own	107	468	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
16	SAMBHUNAGA R COLONY(S.C.- 008)	Fringe area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	97	353	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
17	STATION PARA(S.C.-010)	Core Area	3	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	98	427	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
18	MORICHA PARA(S.C.-018)	Core Area	4	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	103	433	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt



19	POST-OFFICE PARA(S.C.-016)	Fringe area	4	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	55	174	local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficie nt
20	TANTI PARA(S.C.-017)	Core Area	4	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	71	300	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficie nt
21	HARLIAN PALLY(S.C.- 019)	Core Area	5	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	63	323	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficie nt

22	SIBTALLA GHAT(S.C.-020)	Fringe area	6	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	20000	The owner ship of land lies with Own	85	359	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
23	BANDHPUL(S.C.-023)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	106	467	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
24	GURIPARA(S.C.-021)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	77	339	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
25	KASHIGUNJ PAKURTATALLA(S.C.-026)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	91	441	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient



26	KHANSPARA(S C.-025)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	50000	The owner ship of land lies with Own	98	425	Most of the slum dwellers work as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
27	KHASPARA- 2(S.C.-027)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	114	486	Most of the slum dwellers work as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
28	PULDERDHAR( S.C.-024)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	151	639	Most of the slum dwellers work as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
29	SADHAKBAG(S C.-028)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	60000	The owner ship of land lies with Own	126	546	Most of the slum dwellers work as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt

30	SAMSAN PARA(S.C.-022)	Fringe area	7	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	50000	The owner ship of land lies with Own	69	321	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
31	BAHADURPUR (S.C.-030)	Fringe area	8	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	25000	The owner ship of land lies with Own	113	525	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
32	BANNYASROY (S.C.-029)	Fringe area	8	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	15000	The owner ship of land lies with Own	55	261	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
33	DEBIPUR SLUM(S.C.-031)	Fringe area	8	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganj Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	115	504	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt



34	MOHANBAGAN(S.C.-032)	Core Area	8	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	11000	The owner ship of land lies with Own	97	416	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
35	MALLIKBAGAN(S.C.-034)	Core Area	9	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	60000	The owner ship of land lies with Own	73	268	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
36	PHOOLBAGAN(S.C.-035)	Core Area	9	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	106	449	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
37	PUKURPATAL(S.C.-033)	Core Area	9	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	60000	The owner ship of land lies with Own	134	587	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient

38	AMALNAGAR COLONY(S.C.-036)	Core Area	10	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	63	271	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapidated	Water supply is sufficient
39	SAJINAPARA(S.C.-037)	Core Area	10	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	48	208	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapidated	Water supply is sufficient
40	PULKISHOR MALPARA(S.C.-038)	Core Area	11	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	80000	The owner ship of land lies with Own	52	197	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapidated	Water supply is sufficient
41	BARANAGAR-1(S.C.-042)	Core Area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	60000	The owner ship of land lies with Own	110	475	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapidated	Water supply is sufficient



42	BARANAGAR-2(S.C.-043)	Core Area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	24000	The owner ship of land lies with Own	115	474	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
43	BARANAGAR-3(S.C.-044)	Fringe area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	82	348	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
44	BUROSHIBTAL A(S.C.-039)	Fringe area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	15000	The owner ship of land lies with Own	90	355	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
45	LOHAGUNJ(S.C.-041)	Fringe area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	131	572	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient

46	LOHAGUNI-2(S.C.-045)	Fringe area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	105	476	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapi dated	Water supply is sufficient
47	NOYLAXMABAGAN(S.C.-040)	Fringe area	12	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	33000	The owner ship of land lies with Own	112	479	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapi dated	Water supply is sufficient
48	BANSBAGAN(S.C.-047)	Fringe area	13	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	21000	The owner ship of land lies with Own	130	492	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapi dated	Water supply is sufficient
49	GURIPARA(S.C.-046)	Fringe area	13	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	94	409	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch or dilapi dated	Water supply is sufficient



50	GURIPARA-2(S.C.-063)	Fringe area	13	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	60000	The owner ship of land lies with Own	163	668	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
51	BARODUARY-1(S.C.-049)	Core Area	14	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	20000	The owner ship of land lies with Own	53	210	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
52	BARODUARY-2(S.C.-050)	Core Area	14	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	153	557	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
53	BARODUARY-3(S.C.-051)	Fringe area	14	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	115	378	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient

54	BARODUARY-4(S.C.-052)	Fringe area	14	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	10000	The owner ship of land lies with Own	169	605	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
55	NOTUN COLONY(S.C.-048)	Fringe area	14	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	13000	The owner ship of land lies with Own	145	577	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
56	AMAPARA KHASPARA(S.C.-056)	Fringe area	15	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	20000	The owner ship of land lies with Own	120	523	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
57	BHALLADANG A MUSLIMPARA(S.C.-055)	Fringe area	15	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azinganu Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	30000	The owner ship of land lies with Own	94	360	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste, thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient



58	PARSHIPARA(S .C.-054)	Core Area	15	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganu Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	20000	The owner ship of land lies with Own	107	445	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
59	SUKANTAPAL LY(S.C.-053)	Fringe area	15	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganu Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	42000	The owner ship of land lies with Own	104	464	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
60	VELLADANGA MUSLIMPARA- 2(S.C.-062)	Fringe area	15	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganu Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	19000	The owner ship of land lies with Own	105	446	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt
61	BABLABONA(S .C.-057)	Fringe area	16	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj- Azinganu Municipal ity	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The owner ship of land lies with Own	108	448	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The envir onme ntal condi tion in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelli ng units are kacch a or dilapi dated	Water supply is sufficie nt

62	LEBUGAGAN(S.C.-058)	Core Area	16	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	40000	The ownership of land lies with Own	137	553	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient
63	SHIBTALLAGH AT(AZIMGANJ) (S.C.-059)	Core Area	17	Metal road is running in front of the slums	Slum connects it to major areas of Jiaganj-Azimganj Municipality	The National Highway - 34 is 20.0 kms away	More than 20 years	16000	The ownership of land lies with Own	72	215	Most of the slum dwellers works as casual labour in local industries, others engaged in local housekeeping, as sweepers in local areas, as cleaners at Municipal area and as vegetable sellers in nearby areas	The environmental condition in the slum is poor	The slum is partially covered with surface drains but drains are tilted and broken condition resulting clogging	Most of the roads within slums are semi metallic or kuchha road	There is 100% street lights present in the slum	Most of the population adopts unhygienic method for disposing their waste; thereby causing huge damage to health	Most of the dwelling units are kacch a or dilapi dated	Water supply is sufficient



## Section 4 – Description of Proposed Project and Planning

### The Supply Demand Gap and Requirements

#### Particulars

#### Requirements

*Housing: Dwelling Unit provision for Households with standard provisions:*

- ☐ 1 Multipurpose Room
- ☐ 1 Bed Room
- ☐ 1 Kitchen
- ☐ 1 Toilet
- ☐ 1 W.C

*Physical Infrastructure Requirement:*

*Standard Infrastructure Provision for*

- ☐ Water Supply
- ☐ Drainage
- ☐ Roads
- ☐ Electricity

### Project Development Option

In-situ redevelopment and whole of the project will be addressed in the project

### Proposed Development

Based on preliminary understanding, the following components are being proposed

- ☐ Housing Units [Single storied in situ].
- ☐ Standard Physical Infrastructure to be provided in the form of Circulation of Water Supply Drainage, Roads and Electricity

### Innovations proposed in Project Planning

#### Background

Housing activities are known to have the capacity to play a significant role in social-economic development, because they help not only in creation of shelter for the people by also in generating employment opportunities for a large variety skilled and unskilled work force which is a prerequisite for growth and development of settlement. A considerable section of the people without land are in a still worse position as housing schemes for the poor have hitherto been targeted on paper but not applied in practice. Both the serviced land and shelter have become beyond the reach for half of the population-hence formation of slums, encroachments, informal colonies and unauthorized constructions. No land is earmarked for

Economically Weaker Sections and Low Income Groups in Master Plan. The population density norms are required to re-look to enable better utilization of valuable land, as certain areas in the city. This growing slum population and the lack of basic facilities like water and sanitation will badly impact on overall development and prosperity of urban centers like Municipality.

To overcome the existing situation and to promote planned development the following innovative strategies can be adopted for the improvement of the city.

- To ensure that housing, along with the supporting services is treated as a priority and at par with the infrastructure sector.
- Forging strong partnerships between private, public, and cooperative sectors to enhance the capacity of the construction industry.
- Organizing public consultations to meet the special needs of slum dwellers.
- Promotion of livelihood for the slum dwellers.

#### **Financial Implementation:**

#### **Beneficiary led Participation:**

implies development of housing by involvement of Beneficiary

#### **Tasks:**

- ☐ Composition of beneficiaries and organizing the area meetings.
- ☐ Involvement of community and sustainable livelihood framework (SLF) in decision making and prioritization of needs of the slum.
- ☐ Understating of Social-economic profile

#### **Post Project Monitoring**

A Monitoring & Evaluation team has to be formed to know the post project impact on the slums and to document the best practices.

#### **Physical Infrastructure**

##### **Background**

The National Sample Survey Organization (NSSO) in the Ministry of Statistics and Programme Implementation, Government of India has released the report of a nation-wide survey carried out by it during July 2008 to June 2009 (65<sup>th</sup> round) on the condition of urban slums.



The aim of the survey was to collect information on the present condition of the slums and on recent changes, if any, in the condition of facilities available therein. Both 'notified slums' – areas notified as slums by the municipalities, corporations, local bodies or development authorities – and non-notified slums were surveyed – a non-notified slum being any compact urban area with a collection of poorly built tenements, mostly of temporary nature, crowded together usually with inadequate sanitary and drinking water facilities in unhygienic conditions. The present report gives the condition of urban slums, covering ownership, area type, structure, road within and approaching the slum, living facilities like electricity, drinking water, latrine, sewerage, drainage, garbage disposal, and distance of slums from the nearest primary school and government hospital/health centre. It also estimates the proportion of slums where certain specific facilities have improved/ deteriorated over the five years preceding the date of survey.

Comprehensive data on this subject was last collected by NSSO in its 58th round (July - December 2002). The present report provides key indicators from the 58th round as well, for comparison. Some important findings of the survey are given below.

- ☐ About 49 thousand slums were estimated to be in existence in urban India in 2008-09, 24% of them were located along *nallahs* and drains and 12% along railway lines.
- ☐ About 57% of slums were built on public land, owned mostly by local bodies, state government, etc.
- ☐ In 64% of notified slums, a majority of the dwellings were pucca, the corresponding percentage for the non-notified ones being 50%.
- ☐ For 95% slums, the major source of drinking water was either tap or tube wells.
- ☐ Only 1% notified and 7% non-notified slums did not have electricity connection.
- ☐ About 78% of notified slums and 57% of the non-notified slums had a pucca road inside the slum.
- ☐ About 73% notified and 58% non-notified slums had a motorable approach road.
- ☐ About 48% of the slums were usually affected by water logging during monsoon – 32% with inside of slum waterlogged as well as approach road to the slum, 7% where the slum was waterlogged but not the approach road, and 9% where only the approach road was waterlogged in the monsoon.
- ☐ The sanitary conditions in the slums in terms of latrine facility during 2008-09 showed considerable improvement since 2002. Latrines with septic tanks (or similar facility) were available in 68% notified and 47% non-notified slums (up from 66% and 35% respectively in 2002). At the other extreme, 10% notified and 20% non-notified slums (down from 17% and 51% in 2002) did not have any latrine facility at all.
- ☐ About 10% notified and 23% non-notified slums did not have any drainage facility. The



corresponding proportions in 2002 had been 15% for notified and 44% for non-notified slums. Underground drainage systems or drainage systems constructed of pucca materials existed in about 39% notified slums (25% in 2002) and 24% non-notified slums (13% in 2002).

- ☐ Underground sewerage existed in about 33% notified slums (30% in 2002) and 19% non-notified slums (15% in 2002).
- ☐ Government agencies were collecting garbage from 75% notified and 55% non-notified slums. Among these slums, garbage was collected at least once in 7 days in 93% notified and 92% non-notified slums. About 10% notified and 23% non-notified slums did not have any regular mechanism for garbage disposal.
- ☐ Over the last five years, facilities had improved in about 50% of notified slums in terms of roads (both within-slum road and approach road) and water supply. The incidence of deterioration of any of the existing facilities in notified slums during the last five years was quite low (about 6% or below).
- ☐ In case of most slum facilities – sewerage and medical facilities being exceptions – the facility was reported to have improved during the last five years in more than 20% of non-notified slums. Deterioration of any of the existing facilities in non-notified slums, like notified slums, was rare (about 9% or below).
- ☐ Facilities such as street light, latrine, drainage, sewerage and medical facilities were each reported by more than 10% of notified slums to be non-existent both at the time of survey and five years earlier. In case of non-notified slums, facilities like street light, latrine, drainage, sewerage and garbage disposal were each reported by more than 20% of the slums to be non-existent, both during the survey and five years earlier. Where improvement had been brought about during the last 5 years, it was due to the

Government's efforts in about 80-90% of slums, both notified as well as non-notified and for all the facilities. Improvement in educational facilities at primary level was attributed to NGOs in 13% of the notified slums where such improvement was reported. NGOs were also found to have played a role in the improvement of latrine and sewerage system in non-notified slums.

### **Topographical survey and GIS mapping**

The preparation of base map of Wood Industries slum has been prepared with Global Positioning Stations (GPS) and temporary Benchmarks (TBM) for Georeferencing and accurately locating the slum. These points have been selected and located at well defined locations on the ground after discussion with the ULB officials. The existing topographical features have been represented to the actual terrestrial position.

Based on the Total Station survey and Socio-economic survey GIS based thematic maps were generated. This helped in accurate representation of the ground scenario with that of the socio-economic conditions of the



people. The following GIS maps were generated for inclusive planning:

- ☐ Map showing existing Land use Map
- ☐ Map showing Household Size
- ☐ Map showing House Type/Structure, Flooring, Cooking
- ☐ Map showing Minority Status
- ☐ Map showing existing toilet facility
- ☐ Map showing existing road type in front of house
- ☐ Map showing existing source of drinking water
- ☐ Map showing existing source of house lighting

### **Water Supply**

#### **Proposal Rationale**

Water and poverty are inextricably linked. Poor access to water and insufficient sanitation affect the health of the poor, their food security, and their prospects for making a living especially for vulnerable groups, such as children, the elderly, and women in general. Safe and adequate quantities of water and food security are recognized as preconditions for an acceptable development standard.

In almost whole of Asia and the Pacific region - home to nearly 900 million of the world's poorest people - one in three people does not have safe drinking water and one in two lacks adequate sanitation. Water is a critical resource for the poor and plays a key role in many aspects of their livelihoods.

Poor people depend on or are affected by water resources in four key ways:

- ☐ **As direct inputs into production**
- ☐ **For health, welfare, and food security**
- ☐ **For ecosystems viability**
- ☐ **For combating water-related hazards**

Keeping the above in mind, a water scheme for the urban poor needs to be drawn up which shall **Improved Access to Quality Water Services and also** build up institutions accessible to the poor that can efficiently manage water resources. These institutions need to be responsive to the poor and should have an adequate opportunity for the poor to raise their views.

The management of water resources must take place within the wider ecosystems context, and all actions should be based on an understanding of the flows of water resources within river basins and how they affect the poor.

In view of this, the water scheme needs to take into account the following broad objectives:

- ☐ To provide adequate Treated Water
- ☐ To ensure access for the Urban poor
- ☐ To develop institutional framework taking into account the requirements of the Urban Poor

#### **Outcome**

Water is a basic requirement of life. Absence of adequate water is a major issue for health as well as comfort for the poor. With the implementation of the project, the slum dwellers will have access to safe drinking water, which will greatly help their personal health, and hygiene. Quality of life would improve significantly and the multiplier effect due to this investment would reap significant benefit to the economy of this region within a considerable short period of time.

Water supply includes sources of supply, features of collection and distribution system, water demand and availability, quality of surface and groundwater source, reuse and recycling of water including conservation of water at the household level. The endeavour for all the proposals is to optimize the total cost of the system.

#### **Assessment of Overall State of Infrastructure**

In line with the City Development Plan for Kolkata Metropolitan Area (Pg 11-28), it has been resolved that the entire KMA are will be switched over to surface water.

The following norms have been fixed for the region:

- |                                                                    |                 |
|--------------------------------------------------------------------|-----------------|
| <input type="checkbox"/> <b>Kolkata Municipal Corporation Area</b> | <b>200 lpcd</b> |
| <input type="checkbox"/> <b>Howrah Municipal Corporation Area</b>  | <b>150 lpcd</b> |
| <input type="checkbox"/> <b>Municipal &amp; Non-Municipal Area</b> | <b>135 lpcd</b> |

Previously the area was largely dependent on ground water. The status of ground water availability is as follows:

Keeping in mind the reduced rate of aquifer, traces of Arsenic Contamination and presence of Iron on the water, it has been decided to switch over to surface water from River Damodar.

Accordingly, the plant design is adequate to cater to the future requirement of the entire region and no augmentation of supply is required for the present project

#### **Situation Appraisal & Key Intervention for Identified Slum**

Presently accessibility to water supply facilities in the slum pocket is inadequate. The major source of water is from the common tap water available in the slums. The slum is partially connected to the municipal water supply main.

It is now proposed that water pipeline shall be provided in each household with requisite number of



taps, as computed during the survey as felt needs shall be provided under this Project. However, considering that the houses are being provided with water, the provisions of multiple taps have not been encouraged and kept to the minimal level.

Design of distribution system was carried out on the following basis:

- ☐ Population projection
- ☐ Project horizon years
- ☐ Design period for various project components
- ☐ Per capita water supply
- ☐ Factors affecting consumption
- ☐ Existing water supplies
- ☐ Pipeline pressure requirement
- ☐ Supply of water on 24 x 7 basis
- ☐ Economical size of conveying main
- ☐ Choice of pipe materials
- ☐ Peak factor
- ☐ Residual pressure
- ☐ Hydraulic zoning

#### **Design Period for various Project Components**

Water supply projects are designed normally to meet the requirements over a period of 30 years after their completion. The time lag between design and completion of the project should also be taken into account which should not exceed two to five years depending on the size of the project. CPHEEO guidelines have been followed has suggested the design period for various water supply components.

#### **Service Plan**

The pipelines needs to be regularly and kept in full working conditions. It is proposed that operation and maintenance of these pipelines and other assets be done in conjunction with the maintenance programme of the Municipal Corporation. The Bustee Working Committee shall be the first level of responsibility for ensuring that the pipelines etc are kept in good order. The overall operation and maintenance shall be carried out by the project cell of the Municipal Corporation.

#### **Proposed Interventions**

According to the above, the water supply design requirement for Municipality has been fixed at 135 lpcd (Domestic Requirement) + 15% (head loss) +  $100 \times (p^{0.5}) = 163.25$  lpcd (approx).

There is existing water supply scheme which has the capacity for meeting the requirement. Thus there is no additional requirement of any reservoir. There are street stand posts for the slum proposed. But to achieve house connection at slum 100 mm dia. DI pipes are proposed.

The details of water supply lines provide are as follow:

#### **Transmission of Water**

Murshidabad Municipality has water supply through ESR having (24x7) water supply. For the proposed multi-storied buildings sump and pump with OHR is provided for each building. The water supply network for this slum will be connected to the citywide water supply network.

Water supply system broadly involves transmission of water from the water supply main to the area of consumption normally through pipelines. Pipelines normally follow the profile of the ground surface quite closely, normally at 1 metre below ground.

#### **Following design criteria are adopted for this Project:**

- ☐ Gravity pipelines have to be laid below the hydraulic gradient.
- ☐ Pipes are of Ductile Iron, Mild steel, GRP, HDPE, uPVC, Plastic etc.
- ☐ The design of water supply conduits is dependent on pipe friction, available head, velocity allowable, etc.
- ☐ Minimum sizes of 100mm for towns having population up to 50,000 and 150mm for those above 50,000 are recommended.
- ☐ There are a number of formulae available for use in calculating the velocity of flow. However,
- ☐ Hazen William's formula for pressure conduits and Manning's formula for free flow conduits are popularly used.

### **Drainage and Solid waste management**

#### **Proposal Rationale**

The status of adequate Drainage has a close and direct link with environment, water supply and its cleanliness, health and hygiene. The problem of adequate drainage associated with steep influx of population in urban areas, therefore needs to be addressed forth with, debated and deliberated at length, by the policy planners for the development of urban/city areas. Inadequate Drainage results in accumulation of stagnant water and is a major health hazard for the people living in the region.

In the slums there is no proper drainage system and hence stagnation of water is a common occurrence for the slums. In order to improve the situation, there is a need for constructing pucca drains, which will dispose of the stagnant water to the main drains.



## **Outcome**

The proposed drainage system by means of construction of new drains and improvement of existing will help to provide relief to the slum dwellers by means of efficient and effective disposal of storm water through the outfall channels. The outcome of this scheme will by and large enhance the quality of civic life by way of promotion and safeguarding the public health and environmental pollution.

## **Assessment Overall State of Infrastructure**

One of the priority area identified for Wood Industries slum has been absence of adequate drainage. Most of the drainage is kutchra and inadequate for covering the slums which had led to water logging which in turn affected the environment and health of the people on an overall basis.

As mentioned above poor drainage system and consequently chronic water logging are the major issues of concern. There is hardly any pucca drain. The state of drain also affects the condition of the road.

Though there are storm water drains on the main road around the slums, but there is no systematic connection with the internal areas of the slum, thereby leading to acute water logging within the slum. It is worth mentioning that apart from lack of drainage network in several slum pockets, major challenge lies with its maintenance. In numerous cases drains in slums gets choked due to improper disposing of solid waste and other hazardous materials into the existing drains.

Situation gets beyond control particularly during monsoon season like July and August. Accumulated water causes to generate public health problems. Haphazard growth and settlement in the slum area has blocked the natural drainage courses, which in turn causes water logging and stagnation in different parts of the slum.

## **Proposed Interventions**

It is thus proposed to have an integrated drainage programme covering the slum pocket. The programme shall envisage construction of pucca drain throughout the road length and installing a maintenance programme to ensure that the drains are kept free from clogging from plastics and other materials. Depending on the availability of space and requirement, a sections have been designed, Designs of which have been provided in the relevant sections.

## **Road Infrastructure**

### **Proposal Rationale**

A key component of the Proposal is a focused initiative to provide strong connectivity and provision of movement in the slums. This will enable the poor people to benefit from greater mobility and



would increase their employment opportunities, open up trading and marketing of products, and important improve access to health, education, and other social services.

Roads in the slum are highly undeveloped and ill maintained. Poor roads are strong barrier to the development of the slums. Poor road condition and absence of road facility in several slums makes life difficult for all slum dwellers, especially, women and children. It also hampers prompt movement of sick; particularly those who require urgent medical attention. Lack of maintenance, coupled with poor drainage makes life even worse during monsoon season. Road are rarely re-built or re-paired periodically due to several reason. Provision of basic quality road is thus an important element of slum development. The existing road network system of the slum has become inadequate to cope up with the present and ever increasing needs. In order to bear the additional pressure due to enhanced civic, economic and commercial activities of the slum, existing road network system in several places are required either to be up-graded or winded and new roads are also be constructed in a number of places where the network is inadequate.

#### **Proposed status and strategy**

The existing condition of the road is poor and cause great hardship to the slum dwellers particularly women and children. The existing roads in the slum areas are predominantly made of brick pavement. These roads are substantially worn out. The lane roads are Kutcha roads. These roads are highly vulnerable and are in a poor condition particularly in rainy season

One of the major issues is absence of proper maintenance. In view of this it is proposed that the entire road network is to be converted to concrete pavement as concrete pavements are durable and easy to maintain.

The Road needs to be maintained. It is proposed that operation and maintenance and servicing of these roads be done by the Municipality. The Bustee Working Committee shall be the first level of responsibility for ensuring that the pipelines etc. are kept in good order. The project cell of the Municipal Corporation shall carry out the overall operation and maintenance.

#### **Proposed Intervention**

All the proposed roads are rigid pavement-cement concrete roads. Rigid pavements are those which posses note worthy flexural strength. The concrete pavement slab can very well serve as a wearing surface as well as effective base course. Therefore usually rigid pavement structure consists of a cement concrete slab, below which a granular base or sub base course may be provided. Rigid pavements are generally designed and the stresses are analyzed using elastic theory, assuming pavement as an elastic plate resting over elastic or a viscous foundation.

Construction of granular sub-base (GSB) 200 mm thick. Construction of 150 mm thick cement concrete pavement, as per Clause 1501.2.2 M30 (Grade), as per drawing and Technical Specification Clause 1501.



### **Outcome**

After successful implementation of the scheme the slum dwellers will have facilities like pre-school education, adult education, non-formal education and social, recreational activities in the slum area. The community centres would provide the people to gather in, to meet and discuss their problems. It is not just a physical location but a space; where poor people could own, develop their thoughts and also could contribute their own skill and labour to make their dream come true. It will also provide the Municipal Corporation in networking with the urban poor communities in order to exchange information and views.

### **Proposed Intervention**

In view of the above, it is proposed that a Community Centre is established to cater the slum population. For community development a community centre is proposed. The one storied community centre has total plinth area of 223.4 sq m.

There will be multipurpose hall which may be used as skill development centres or livelihood centre, health centres and Crèche are provided.

The Community Centres act mainly as a supporting unit for livelihood and for revenue generation for O&M.

### **Materials of construction:**

- ☐ PCC (1:3:6) for foundation
- ☐ RCC M-20 for substructure & superstructure (Column, Beam, Slab)
- ☐ HYSD Steel
- ☐ 1<sup>st</sup> Class Brick Masonry
- ☐ 1:6 (Cement: Sand) plaster – 10 mm on soffit of beam & slab, 15 mm on internal walls & 20 mm on external walls
- ☐ IPS flooring

### **Definition of Slum for Housing**

Different definitions of a slum exist in different statutes and in urban poverty literature. For the purpose of HOUSING SCHEME, it is proposed to adopt the definition given in the 2001 Census, which is as follows:

- a. All areas notified as 'Slum' by State/Local Government and UT Administration under any Act;
- b. All areas recognized as 'Slum' by State/Local Government and UT Administration, which have not been formally notified as slum under any Act;

**'Slum' or 'Slum Area'** – is a compact settlement of at least 20 households ( For NE & Special Category States it is 10-15 households) with a collection of poorly built tenements, mostly of temporary nature, crowded together usually with inadequate sanitary and drinking water facilities in

unhygienic conditions.

### Situation Appraisal

The people living in the slums mostly have kutchha (10) and semi-pucca (186) housing. In certain cases where pucca housing is available, they are usually in dilapidated condition. The kutchha houses are in very poor condition and require extensive repairs. Most of the houses have tiles on roof. While during the survey some of the houses have been noted to be in average condition, the quality of these houses is also speedily deteriorating.

### Proposed Intervention

In line with the vision to Housing for All an integrated housing programme is proposed to be implemented. The target will be all the slum dwellers in the pocket. In situ single dwelling units are proposed.

**Table-14: Dwelling units**

Building type	Number of DU
Beneficiary Led Self Construction	877 within 63Nos. Of slums and 600 in non slum area

### Building Plan

The buildings are proposed to cover an area of approximate 32 Sq.mt along with provision of 2 rooms, kitchen and sanitation facility. The layout, size and type design of housing dwelling units depends on the local conditions and the preferences of the beneficiary. The houses, has been designed in accordance with the desire of the beneficiaries, keeping in view the climatic conditions and the need to provide ample space, kitchen, ventilation, sanitary facilities, etc. and the community perceptions, preferences and cultural attitudes. In line with the scheme, carpet area of the house will be not less than 25 sq. mts and preferably two room accommodation plus kitchen and toilet should be constructed.

### Building material

- ☐ PCC (1:3:6) for foundation
- ☐ RCC M-20 for substructure & superstructure (Column, Beam, Slab)
- ☐ HYSD Steel
- ☐ 1<sup>st</sup> class Brick Masonry
- ☐ 1:6 (Cement: Sand) plaster – 10 mm on soffit of beam & slab, 15 mm on internal walls & 20 mm on external walls



- ☐ IPS flooring

### **Structural Design**

- ☐ Following are the general considerations in the analysis/design.
- ☐ For all structural elements, M20 grade concrete and Fe 415 grade of steel is used.
- ☐ Plinth beams passing through columns are provided as tie beams.
- ☐ Pedestals are proposed up to ground level.
- ☐ Beam Centre-line dimensions are followed for analysis and design.
- ☐ For all the building, walls of 250 mm and 125mm thick with 20 mm External plaster and 12 mm thick internal plaster are considered.
- ☐ Seismic loads are considered acting in the horizontal direction along either of the two principal directions.

### **Design data**

- ☐ Live load: 2.0 kN/m<sup>2</sup> at typical floor
- ☐ 1.5 kN/m<sup>2</sup> on terrace (With Access) : 0.75 kN/m<sup>2</sup> on terrace (without Access)
- ☐ Floor finish 50mm (0.05\*24) = : 1.2 kN/m<sup>2</sup>
- ☐ Ceiling plaster 12mm (0.012\*20.8) : 0.25 kN/m<sup>2</sup>
- ☐ Partition walls (Wherever Necessary) : 1.0 kN/m<sup>2</sup>
- ☐ Terrace finish: 1.5 kN/m<sup>2</sup>
- ☐ Earthquake load: As per IS-1893 (Part 1) - 2002
- ☐ Depth of foundation below ground: ,0.7 m
- ☐ Walls: 250 mm thick brick masonry walls at external and 125mm walls internal.

### **Reference codes:**

- ☐ IS 456: 2000 - Code of practice -Plain and Reinforced concrete.
- ☐ IS :1893 :2002 - Criteria for Earthquake resistant design of structures(Part-1)
- ☐ IS: 13920: 1993 - Ductile detailing of Reinforced concrete structures subjected to seismic forces.
- ☐ SP: 34 - Hand Book on Concrete Reinforcement and Detailing.
- ☐ S: 875: 1987 - Code of practice for design loads (other than earthquake) for buildings and structures. (Part-2)

### **Identification of Beneficiaries**

Municipality Municipal Corporation, in consultation with State Urban Development Agency (SUDA), will approve the phasing of the beneficiaries in the region. The beneficiaries so identified and the projects so prepared shall be done in consultation with the committees and community development societies already existing in that particular city. The identification of beneficiaries will be on the basis of the baseline survey

already conducted under PMAY Demand Survey.

### **Allotment of Houses**

Allotment of dwelling units will be in the name of the female member of the household. Alternatively, it can be allotted in the name of husband and wife jointly. Ownership of land required for every Beneficiary.

### **Town Planning Norms**

Up-gradation of existing constructions and construction of new houses shall only be taken after approval of the lay out by the urban local body. Respective State Govts. may relax some town planning norms for sanction of such layout Plans, to facilitate HOUSING SCHEME, however, minimum acceptable standards of Town Planning will need to be set and followed.

All planning are done as per UDPFI & CPHEOO guidelines and local Municipal Bye-laws.

### **Compliance with Municipal Bye laws**

All designs & drawings are created keeping in line with the municipal bye laws.

### **Tenure**

Unlike rural areas, land is scarce in urban areas particularly in large metropolises. Under HOUSING SCHEME, the responsibility for providing land for the project rests with the State Government or its agencies.

### **Summary of Investment**

#### **Project Costing**

The costing for the individual sectors has been made on the basis of applicable Schedule of Rates. The details of each of the sub-projects have been provided in the respective sections.

#### **The cost components include:**

Infrastructure: Cost of infrastructure development/up-gradation including water supply, sewerage, storm water drainage, solid waste management, roads & drainage, street lights, etc.

Housing: Construction Costs would need to be arrived from the various components that are proposed to be implemented and would vary depending on the development option identified.

#### **GOI Contribution:**

PMAY scheme guidelines stipulate that, 1.5 lakhs of the unit cost of dwelling unit.

The Central share would be available as per milestones set out in Memorandum of Agreement (MoA).



**Beneficiary Contribution:**

In order to ensure beneficiaries interest, financial contribution by the beneficiaries is critical.. The share of beneficiary contribution in housing is proposed to be a minimum of 25000/-. As per PMAY guidelines no contribution from the beneficiaries is expected in infrastructure improvements

**State Contribution:**

The decision would be left to the remaining share would have to be arranged by the State. State will contribute 5% of total Dwelling cost for infrastructure.

**ULB Contribution:**

ULB have no contribution on dwelling unit cost. ULB will contribute 5% of total Dwelling cost for infrastructure.

In the 1<sup>st</sup> Meeting of SLSMC of West Bengal it has been decided that the flowing funding pattern should be adopted for implementation of PMAY until further revision.

**Table-15: Share of Fund**

Type of City/Towns as per 2011 census	Component	Contribution of			
		Central Rs.(Lakhs)	State Rs.(Lakhs)	ULB Rs.(Lakhs)	Beneficiaries Rs.(Lakhs)
Total cost of Beneficiary LED Construction	Housing	1.5	1.93	Nil	0.25
	Infrastructure	Nil	5 %	5 %	Nil

**4.2. Disaster Management and Mitigation**

Most of the citizens admit the necessity of elimination of hazards arising out of collapse of ill maintained buildings of temporary nature during periods of heavy rains and storms and immediate renovation of drainage system by construction of drains of adequate size and re-sectioning of the channels for increasing their carrying capacities by following appropriate design for the same. The structural design of the building is made by the MED, Govt. of West Bengal considering the norms of disaster management.

### 4.3. Statutory approval including environmental clearance (as applicable)

**Table-16: Statutory approval including environmental clearance**

IMPACT & REMEDIES		
1.	Utilization of alternative material Characteristics and availability of alternative material	Locally available bricks etc. will be used.
2.	Rehabilitation of water bodies & measures for maintaining surface runoff smoothly	No water body is affected by the alignment of road. The road side open C. C. / Brick masonry drains have been provided for free flow of storm water.
3.	Measures for Erosion Control	Not applicable for the slum area.
4.	Conservation of Topsoil a. Extent of loss of topsoil b. Area requirement for topsoil conservation c. Inclusion of conservation of topsoil	Not applicable for the slum area.
5.	Impact on Heritage & Culture a. Identification of locally significant cultural properties b. Assessment of likely impacts on each cultural property due to project implementation c. Possible measures for avoidance i) Identification of alternative routes ii) Relocation of Culture property in consultation with the local community iii) Common Property	Question does not arise.
6.	Location of Natural Habitants	It will not be disturbed
7.	Construction of site office / Camp	Temporary construction of camp / office shall be established by contractor and since the project is small and scattered, the temporary impact on environment for Construction Camp / office at the time of execution of work is negligible.
8.	Quarrying of Materials a. Sourcing of materials from quarries b. Lead from various existing quarries c. Adequacy of material for the project in these quarries	The construction materials require for the project shall be procured from : a) Stone metal: from the existing. b) Bricks: From the existing brick fields nearby the project site. c) Sand: From the nearest source. All the materials are sufficiently available.
9.	Water Requirement; Identification of potential sources of water	Water required for the construction of work will be available from ground water. There is no scarcity of water in the region.



10.	Location of Waste Water Disposal :	
	a. Location for disposal of waste water	The surface drain have been proposed in the slum for disposal of waste water.
	b. Outfalls locations for longitudinal drains	
	i) Outfall level and back flow	Natural slope of the ground will be maintained for waterways for discharge of surface runoff. No possibility of back flow except in the case of heavy flood.
	ii) The outfall is in natural stream; measures shall be taken to prevent sediment into the stream.	The storm water drain of the slums will discharge the water to the main high drain of the town.
11.	Air Pollution during construction work	Work shall be carried out by equipments like concrete mixer machine vibrator etc. at this time of concerting work only for which air pollution will be negligible.
12.	Identify locations susceptible to induced development	Locations vulnerable to induced development: In such location the Municipality has committed not to allow building construction activity. a. Lands within 50 m of junctions b. Agricultural lands with enforce restriction on building activity on either side of road. Stretches within 100m of worship places, weekly fairs and locations of community mass gatherings.
13.	Roles and responsibilities of municipality in regulating development	The municipality shall lay down restrictions on building activities along the by-pass roads : 1. Municipality will enforce restriction on building activity on either side of road. 2. Development of Residential sites outside Existing Settlement. Appropriate measure towards the removal of encroachments onto the public land to be taken.
14.	Traffic Congestion and related air & noise pollution	As the road passes through the slum area of the town and two wheelers, Three wheelers, light vehicle will move hence there will not be any traffic congestion, related air & noise pollution.
15.	Opportunity in economic activities due to ease of transportation system	The benefits due to this project are : 1. Generation of Man days 2. Improvement in Household or population sector i.e. Improvement of personal health, hygiene, socio-economic condition, education etc.



## Section 5 – Project Cost Estimate

### 5.1. Abstract cost estimates

#### 5.1.1 Component wise abstract for each slum and non-slum

Table-17: Component wise abstract for each slums and non-slums area

SLUM AND NON SLUM WISE DETAILS OF DU AND INFRASTRUCTURE COST OF 17						2016-
SL. NO	SLUM/ NON- SLUM NAME	DWELLI NG UNIT	INFRASTRUCTURES			
			Cost involved @ Rs. 3.68 Lakhs per DU.	ROADS (In Meter)	Cost involved @ Rs. .04097 lakh per meter	Total
	DHAKAPARA(S.C.-001)	4	14.72	36	1.47	16.19
	ENATULIBAGH COLONY(S.C.-060)	2	7.36	18	0.74	8.10
3	ENATULIBAGH MUSLIMPARA(S.C.-061)	3	11.04	27	1.10	12.14
4	ENATULIBAGH(S.C.-005)	45	165.60	404	16.56	182.16
5	NUTON COLONY(S.C.-002)	14	51.52	126	5.15	56.67
6	PAHARIA PARA(S.C.-004)	11	40.48	99	4.05	44.53
7	PROMOD COLONY(S.C.-003)	25	92.00	225	9.20	101.20
8	MAHAJAN PATTY(S.C.-006)	11	40.48	99	4.05	44.53
9	BHALLADANGA-1(S.C.-007)	2	7.36	18	0.74	8.10
10	BHALLADANGA-2(S.C.-014)	2	7.36	18	0.74	8.10
11	BHALLADANGA-3(S.C.-015)	14	51.52	126	5.15	56.67
12	BHANGAN PARA(S.C.-011)	5	18.40	45	1.84	20.24
13	CHAIPARA(S.C.-009)	35	128.80	314	12.88	141.68
14	DAS PARA(S.C.-013)	6	22.08	54	2.21	24.29
15	GUHAPARA(S.C.-012)	15	55.20	135	5.52	60.72
16	SAMBHUNAGAR COLONY(S.C.-008)	19	69.92	171	6.99	76.91
17	STATION PARA(S.C.-010)	22	80.96	198	8.10	89.06
	MORICHA PARA(S.C.-018)	15	55.20	135	5.52	60.72
19	POST-OFFICE PARA(S.C.-016)	6	22.08	54	2.21	24.29
20	TANTI PARA(S.C.-017)	7	25.76	63	2.58	28.34
21	HARIJAN PALLY(S.C.-019)	4	14.72	36	1.47	16.19
22	SIBTALLA GHAT(S.C.-020)	16	58.88	144	5.89	64.77
23	BANDHPUL(S.C.-023)	16	58.88	144	5.89	64.77
24	GURIPARA(S.C.-021)	28	103.04	252	10.30	113.34
25	KASHIGUNJ PAKURTATAL(S.C.-026)	18	66.24	162	6.62	72.86
26	KHANSPARA(S.C.-025)	17	62.56	153	6.26	68.82
27	KHASPARA-2(S.C.-027)	14	51.52	126	5.15	56.67
28	PULDERDHAR(S.C.-024)	14	51.52	126	5.15	56.67
29	SADHAKBAG(S.C.-028)	22	80.96	198	8.10	89.06
30	SAMSAN PARA(S.C.-022)	25	92.00	225	9.20	101.20
31	BAHADURPUR(S.C.-030)	19	69.92	171	6.99	76.91
32	BANNYASROY(S.C.-029)	7	25.76	63	2.58	28.34
33	DEBIPUR SLUM(S.C.-031)	42	154.56	377	15.46	170.02
34	MOHANBAGAN(S.C.-032)	24	88.32	216	8.83	97.15



35	MALLIKBAGAN(S.C.-034)	8	29.44	72	2.94	32.38
36	PHOOLBAGAN(S.C.-035)	17	62.56	153	6.26	68.82
37	PUKURPATAL(S.C.-033)	13	47.84	117	4.78	52.62
38	AMALNAGAR COLONY(S.C.-036)	22	80.96	198	8.10	89.06
39	SAJINAPARA(S.C.-037)	2	7.36	18	0.74	8.10
40	PULKISHOR MALPARA(S.C.-038)	8	29.44	72	2.94	32.38
41	BARANAGAR-1(S.C.-042)	8	29.44	72	2.94	32.38
42	BARANAGAR-2(S.C.-043)	13	47.84	117	4.78	52.62
43	BARANAGAR-3(S.C.-044)	7	25.76	63	2.58	28.34
44	BUROSHIBTALA(S.C.-039)	8	29.44	72	2.94	32.38
45	LOHAGUNJ(S.C.-041)	14	51.52	126	5.15	56.67
46	LOHAGUNJ-2(S.C.-045)	11	40.48	99	4.05	44.53
47	NOYLAXMABAGAN(S.C.-040)	10	36.80	90	3.68	40.48
48	BANSBAGAN(S.C.-047)	6	22.08	54	2.21	24.29
49	GURIPARA(S.C.-046)	23	84.64	207	8.46	93.10
50	GURIPARA-2(S.C.-063)	9	33.12	81	3.31	36.43
	BARODUARY-1(S.C.-049)	4	14.72	36	1.47	16.19
52	BARODUARY-2(S.C.-050)	28	103.04	252	10.30	113.34
53	BARODUARY-3(S.C.-051)	7	25.76	63	2.58	28.34
54	BARODUARY-4(S.C.-052)	30	110.40	269	11.04	121.44
55	NOTUN COLONY(S.C.-048)	17	62.56	153	6.26	68.82
56	AMAIPARA KHASPARA (S.C.-056)	14	51.52	126	5.15	56.67
57	BHALLADANGA MUSLIMPARA(S.C.-055)	7	25.76	63	2.58	28.34
58	PARSHIPARA(S.C.-054)	5	18.40	45	1.84	20.24
59	SUKANTAPALLY(S.C.-053)	35	128.80	314	12.88	141.68
60	VELLADANGA MUSLIMPARA-2(S.C.-062)	6	22.08	54	2.21	24.29
61	BABLABONA(S.C.-057)	3	11.04	27	1.10	12.14
62	LEBUGAGAN(S.C.-058)	6	22.08	54	2.21	24.29
63	SHIBTALLAGHAT (AZIMGANJ) (S.C.-059)	7	25.76	63	2.58	28.34
<b>Total in Slum</b>		<b>877</b>	<b>3227</b>	<b>7877</b>	<b>323</b>	<b>3550</b>
1	Ward 1	32	117.76	287	11.78	129.54
2	Ward 2	34	125.12	305	12.51	137.63
	Ward 3	22	80.96	198	8.10	89.06
4	Ward 4	84	309.12	755	30.91	340.03
5	Ward 5	32	117.76	287	11.78	129.54
6	Ward 6	18	66.24	162	6.62	72.86
7	Ward 8	5	18.40	45	1.84	20.24
8	Ward 9	38	139.84	341	13.98	153.82
9	Ward 10	26	95.68	234	9.57	105.25
10	Ward 11	5	18.40	45	1.84	20.24
11	Ward 12	81	298.08	728	29.81	327.89
12	Ward 13	72	264.96	647	26.50	291.46
13	Ward 14	18	66.24	162	6.62	72.86
14	Ward 15	41	150.88	368	15.09	165.97
15	Ward 16	54	198.72	485	19.87	218.59
16	Ward 17	38	139.84	341	13.98	153.82
<b>Total in Non Slum</b>		<b>600</b>	<b>2208</b>	<b>5389</b>	<b>221</b>	<b>2429</b>
<b>Total</b>		<b>1477</b>	<b>5435.36</b>	<b>13266.68</b>	<b>543.54</b>	<b>5978.90</b>



## 5.2. Detailed Estimates

## 5.2.1. Detailed Estimate of Provision of Housing

Table-18: Detailed Estimate of Provision of Housing

<b>DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE</b> <b>Pradhan Mantri Awas Yojana Housing For All (Urban)</b> <b>Total Covered Area- 32.38 sq.m (With Electrical Works)</b> Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building & Sanitary w.e.f-01.07.2014 & Corrigenda <b>Floor Area 25.77 sqm</b>					
SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
1	Earthwork in excavation in foundation trenches or drains, in all sorts of soil ( including mixed soil but excluding laterite or sandstone) including removing spreading or stacking the spoils within a lead of 75 m as directed including trimming the sides of trenches, levelling, dressing and ramming the bottom, bailing out water etc. as required complete. a) Depth of excavation not exceeding 1500mm .	13.000	%cu.m.	12047.00	1566.11
	<b>SOR, PWD, P-1, I -2 a</b>				
2	Earth work in filling in foundation trenches or plinth with good earth in layers not exceeding 150 mm. including watering and ramming etc. layer by layer complete.( Payment to be made on the basis of measurement of finished quantity of work )				
	a) With earth obtained from excavation of foundation.	11.120	%cu.m.	7831.00	870.81
	<b>SOR, PWD, P-1, T/3 a</b>				
3	Supplying Laying Polithin Sheets etc. <b>SOR, PWD, P-45, T - 13</b>	22.000	sqm	25.00	550.00
4	Cement concrete with graded Stone ballast (40 mm.) excluding shuttering.a) In ground floor and foundation.6 : 3 : 1 proportion Pakur variety <b>SOR, PWD, Page 24 ; Item -10 a</b>	3.500	cu.m.	5823.00	20380.50
5	25 mm. thick damp proof with cement concrete (4:2:1) (with graded stone aggregate 10 mm. Normal size) and painting the top surface with a coat of bitumen using 1.7 kg. per sq.m. including heating the bitumen and cost and carriage of all materials complete.	6.810	sqm,	297.00	2022.57
	<b>SOR, PWD, P-45, T-12</b>				
6	Brick work with 1st class bricks in cement mortar				



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.58 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.47 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
	(6:1)				
	a) In foundation and plinth.	10.430	cum	5719.00	59649.17
	b) In super structure	15.240	cum	5943.00	90571.32
	<b>SOR, PWD, P-29, T -22(a), (b)</b>				
7	125mm thick brick work with 1st. class bricks in cement mortar (4:1). a) In ground floor <b>SOR, PWD, P-73, I -29</b>	23.220	sq.m.	783.00	18181.26
8	Ordinary Cement concrete (mix 1:1.5:3) with graded stone chips (20 mm nominal size) excluding shuttering and reinforcement if any, in ground floor as per relevant IS codes. (i) Pakur Variety <b>SOR, PWD, P-14, T -7(i)</b>	3.940	cu.m.	6851.66	26995.54
9	Reinforcements for reinforced concrete work in all sorts of structures including distribution bars, stirrups, binders etc. including supply of rods, initial straightening and removal of loose rust (if necessary), cutting to requisite length, hooking and bending to correct shape, placing in proper position and binding with 16G black annealed wire at every inter-section, complete as per drawing and direction. (a) For works in foundation, basement and upto roof of ground floor / upto 4m. (i) Tor steel/Mild steel. <b>SOR, PWD, P-27, T -15(i)</b>	0.309	MT	60705.93	18775.74
10	Hire and labour charges for shuttering with centreing and necessary staging upto 4 m. using approved stout props and thick hard wood planks of approved thickness with required bracing for concrete slabs, beams, columns, lintels curved or straight including fitting, fixing and striking out after completion of works. (upto roof of ground floor). (When the height of a particular floor is more than 4 m. the equivalent floor ht. shall be taken as 4 m. and extra for works beyond the initial 4 m. ht. shall be allowed under 12(e) for every 4 m. or part thereof.) <b>SOR, PWD, P-66, T -12(a)</b>				
	25 mm. to 30 mm. thick wooden shuttering as per	37.063	M <sup>2</sup>	360.00	13342.68



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.58 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.37 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
	decision & direction of Engineer-in-charge. Ground Floor				
11	Plaster ( to wall, floor, ceiling etc.) with sand and cement mortar including rounding off or chamfering corners as directed and raking out joints or roughening of concrete surface, including throating, nosing and drip course where necessary . In ground floor. A) With 6:1 cement mortar. a) Inside wall 20 mm thick plaster <b>SOR, PWD, P-151, T -2 (i)(b)</b>	116.940	sq.m.	181.00	21166.14
	b) Out side Wall, 15mm th. <b>SOR, PWD, P-151, I -2 (i)(c)</b>	111.950	sq.m.	156.00	17464.20
	B)10mm th ceiling plaster (4:1) <b>SOR, PWD, P-151, I -2 (i)(c)</b>	23.330	sq.m.	140.00	3266.20
12	Neat cement punning about 1.5mm thick in wall, dado, window, sills, floor, drain etc. <b>SOR, PWD, P-152, I -8</b>	26.700	sq.m.	38.00	1014.60
13	Artificial stone in floor,dado, staircase etc. with cement concrete (4:2:1) with stone chips laid in panels as directed with topping made with ordinary or white cement (as necessary) and marble dust in proportion (2:1) including smooth finishing and rounding off corners and including application of cement slurry before flooring works, using cement @ 1.75 kg./sq.m. all complete including all materials and labour. In ground floor. 3 mm. thick topping (High polishing grinding on this item is not permitted) with ordinary cement. 20mm thick <b>SOR, PWD, P-40, I -3 (i)</b>	26.490	sq.m.	265.00	7019.85
14	Supplying, fitting & fixing MS clamp for fixing door and window frame made of flat bent bar, end bifurcated, fixed in cement concrete with stone chips (4:2:1)a fitted and fixed omplete as per direction. 40mm x 6mm x 125 mm length. (Cost of cement concrete will be paid separately) <b>SOR, PWD, P-90, I -18 (c)</b>	34	each	22.00	748.00



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.58 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.37 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
15	Wood work in door and window frame fitted and fixed complete including a protective coat of painting at the contact surface of the frame other Local wood <b>SOR, PWD, P-85, T -1(i)</b>	0.213	cu.m.	46171.00	9834.42
16	Panel Shutter of door & Window (each Panal Consisting Of single Plan without Join) 25 mm thick shutter with 12 mm thick Panal of size 30 to 45 cm. Other Local wood <b>SOR, PWD, P-105, I -84 (iv)c</b>	8.520	sq.m.	1567.00	13350.84
17	Iron butt hinges of approved quality fitted and fixed with steel screws, with ISI mark. a)75mm x 47mm x 1.70mm <b>SOR, PWD, P-91, T -20(iv)</b>	32.000	each	34.00	1088.00
18	Iron Socket Bolt of approved quality fitted and fixed complete. i) 150 mm long x 10 mm dia <b>SOR, PWD P-93, I-25,c</b>	11.000	each	71.00	781.00
19	White washing including cleaning and smoothening surface thoroughly (5 parts of stone lime and 1 part of shell lime should be used in the finishing coat). Two Coats <b>SOR, PWD, P-155, I -3 (b)</b>	124.960	%sq.m.	1887.00	2358.00
20	Colour washing with ella with a coat of white wash priming including cleaning and smoothening surface thoroughly external surface One Coat <b>SOR, PWD, P-155, I - 4(ii)(a)</b>	100.560	%sq.m.	1514.00	1522.48
21	Priming one coat on timber, plastered or on steel or other metal surface with synthetic enamel/oil bound primer of approved quality including smoothening surfaces by sand papering etc.				
	1) On timber surface <b>SOR, PWD, P - 162, I - 7(a)</b>	21.690	sq.m.	41.00	889.29
	2) On Steel Surface <b>SOR, PWD, P - 162, I - 7(b)</b>	2.700	sq.m.	31.00	83.70



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.68 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.37 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
22	Painting with best quality synthetic enamel paint of approved make and brand including smoothening surface by sand papering etc. including using of approved putty etc. on the surface, if necessary : With super gloss (hi-gloss)-With any shade except white.				
	a) On timber or plastered surface Two Coats	21.690	sq.m.	89.00	1930.41
	b) On Steel surface Two Coats	2.700	sq.m.	86.00	232.20
	<b>SOR, PWD, P - 162, - 8A(aii),(bii)</b>				
23	Iron hasp bolt of approved quality fitted and fixed complete (oxidised) with 16 mm dia with center bolt and round fitting. 300 mm long <b>SOR, PWD, P-93, I - 27c</b>	2.000	each	193.00	386.00
24	Precast piered concrete jally work as per design and manufacture's specification including moulding etc. with stone chips and necessary reinforcement shuttering complete including fitting, fixing in position in all floors. (a) 37.5 mm th. panels Cement & steel required for this item <b>will not be</b> issued by deptt. <b>SOR, PWD, P-32, I - 38 (b)</b>	1.690	sq.m.	351.00	593.19
25	Supplying, fitting and fixing UPVC down pipes A type and fittings conforming to IS 13592-1992 with necessary clamps nails including making holes in walls, etc. and cutting trenches in any soil, through masonry concrete structure etc. if necessary and mending good damages including jointing with jointing materials ( Spun yarn, valamoid / bitumen / M. seal etc.) complete. <b>P-173, I-21 A (ii), C(ii), D(ii)</b> <b>SOR, PWD, P173, I - 21 A (ii), C(ii), D(ii)</b>				
	i) UPVC Pipe 110 mm dia	3.000	Mtr.	291.00	873.00
	ii) UPVC Bend 87.5 degree 110 mm dia	2.000	each	162.00	324.00
	iii) UPVC Shoe 110 mm	1.000	each	128.00	128.00
26	M.S.or W.I. Ornamental grill of approved design joints continuously welded with M.S, W.I. Flats and bars of windows, railing etc. fitted and fixed with necessary screws and lugs in ground floor.	0.284	Qntl	8247.00	2342.15



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.33 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.37 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
	Grill weighing 10 kg/sq m to 16 kg/m <sup>2</sup> <b>SOR, PWD, P - 76, I - 10 (i)</b> (2.70sqm @ 10.5kg per sqm = 28.35 kg)				
27	Shallow water closet Indian pattern(I.P.W.C.) of approved make in white vitreous chinaware supplied, fitted and fixed in position (excluding cost of concrete for fixing). 450 mm long <b>SOR, PWD, (Sanitary) P - 65, I - 1 (iii)</b>	1.000	each	1062.00	1062.00
28	Foot rest for water closet of size 275 mm X 125 mm with Artificial stone(4:2:1) with 6 mm stone chips and chequered including adding colour as necessary. <b>SOR, PWD, (Sanitary) P - 66, I - 9</b>	1.000	Pair	70.00	70.00
29	Supplying, fitting and fixing cast iron 'P' or 'S' trap conforming to I.S. 3989 / 1970 and 1729 / 1964 including lead caulked joints and painting two coats to the exposed surface. S Trap 100 mm <b>SOR, PWD, (Sanitary) P - 54, I - 14(B-iii)</b>	1.000	each	923.00	923.00
30	Supplying, fitting fixing CI Round Gratings 150mm dia <b>SOR, PWD, (Sanitary) P - 55, I - 18(ii)</b>	1.000	Each	100.00	100.00
	Construction of 2 circular leach pit of inside diameter 1000 mm. & a depth of 1000 mm. With a layer of 250 mm. Thick brick work with cement mortar (6:1) & honeycombed brick wall (4:1) at every alternate layer upto a height of 925 mm. From bottom and then 125 mm. thick brick wall (4:1) for a height of 300 mm. and covered with 75mm. RCC slab (4:2:1) with 8mm tor steel @ 150 mm. centre to centre both ways including plastering and neat cement punning on top of the slab and making hooking arrangement on slab for lifting of the slab if require as well as jointing the connection with the inspection pit (450 x 450) covered with 50mm thick RCC slab (4:2:1) with stone chips and necessary reinforcement and connected with 100 mm dia PVC pipe laid over rammed earth and then covered the pipe properly	1	Item	7544.00	7544.00



**DETAILED ESTIMATE FOR THE CONSTRUCTION OF SINGLE UNIT DWELLING HOUSE****Pradhan Mantri Awas Yojana Housing For All (Urban)****Total Covered Area- 32.58 sq.m (With Electrical Works)**

Reference of Schedule of Rates : PWD (W.B.), Schedule of Rates Building &amp; Sanitary w.e.f-01.07.2014 &amp; Corrigenda

**Floor Area 25.37 sqm**

SL No.	Description of Works	Quantity	Unit	Rate (Rs.)	Amount (Rs.)
	with powder earth including supplying fitting fixing fibre glass pan P-tap & polythene pipe as per requirement to connect with the inspection pit complete with all respect as per direction of EIC.(ANNEXURE-II)				
	<b>TOTAL AMOUNT</b>		<b>Rs.</b>		<b>350000.36</b>
	<b>Say</b>		<b>Rs.</b>		<b>350000.00</b>
	<b>Add for Electrical Works (ANNEXURE-I)</b>		<b>Rs.</b>		<b>17858.00</b>
	<b>TOTAL AMOUNT</b>		<b>Rs.</b>		<b>367858.00</b>
	<b>Say</b>		<b>Rs.</b>		<b>368000.00</b>
<b>(Rupees Three lakh Sixty eight thousand only)</b>					

Table-19: ESTIMATE FOR ELECTRICAL WORKS FOR ONE DWELLING UNIT UNDER PMAY

<b>ESTIMATE FOR ELECTRICAL WORKS FOR ONE DWELLING UNIT UNDER PMAY (ANNEXURE-I)</b>					
Sl No.	Item of works	Unit	Rate	Quantity	Amount
1	Supplying & fitting polythene pipe complete with fittings as necessary. Under ceiling /beam/bound with 22SWG GI wire inclusive S & Drawing 1x18 SWG GI wire as fish wire inside the pipe & fittings and providing 55 mm dia disc of MS sheet (20SWG) having colour paint at one face first ended at the load point end of the polythene pipe with fish wire (synchronizing with roof/beam casting work of building construction) 19 mm dia 3 mm thick polythene pipe	RM	39.00	25.00	975.00
2	Powerckt wiring supplying and drawing 1 ; 1KV grade single core stranded FR PVC insulated & unseathed single core stranded Copper wire (Finolex make) 2 x 2.5 sqmm (PH & N) +1x1.5 sqmm (ECC) per laid polythene pipe and by the pre-laid GI fish wire & making necessary connections as required.	RM	76.00	50.00	3800.00
3	Concealed Distribution wiring in in 2x1.5 sqmm single core standard *FR* insulated and unseathed copper wire Finolex make & 1x1.5 sq mm single core stranded PVC insulated and unseathed copper wire (Finolex make) wire used as ECC in 19 mm bore 3	points	828.00	10.00	8280.00

Chief Engineer  
M E Directorate  
Dept. of Municipal Affairs  
Govt. of West Bengal



	mm thk. polythene pipe complete with all accessories embedded in wall smooth run to light / fan/call bell point with piano key type switch (6 Amps) (Anchor make) fixed on sheet metal (16 SWG) Switch Board with bakelite/ perspex (wall matching colour) Top cover (3 mm thick) flushed in wall including mending all good damages to original finish Average per point 6.00 mt.				
4	Deistribution concealed wiring with 2x1.5 sq mm (PH & N) single core stranded FR PVC insulated & unsheathed single core stranded 1.1 KV grade Copper Wire (finolex) & 1x1.5 sq mm (ECC) single core stranded (PH & N) 1.1 KV grade cu wire (finolex) & 1 x 1.5 sq mm single core stranded PVC insulated & unsheathed cu wire (finolex) used as ECC in 19 mm bore, 3 mm thick polythene pipe complete with all accessories embedded in wall 250 volt 5 amp 3 pin plug point including S & F 250 Volt 5 amp 3 pin flush type plug socket & piano key type switch (Anchor make) on existing switch board as mentioned sl. no.3	points	76.00	2.00	152.00
5	Supplying & drawing 1.1 KV grade single core stranded FR PVC insulated & unseathed single core stranded cu Wire 3x2.5 sq mm (finolex make) in the prelaid polythene pipe & by the prelaid GI fishwire & making necessary connection as required (CESC supply to consumer DP near to CESC & inside the room another DP near CESC & inside the room another DP of dwelling units)	RM	86.00	15.00	1290.00
SLNo.	Item of works	Unit	Rate	Quantity	Amount
6	Supplying Delivery & instalation on wall of 30/32 amp DP MCB of Havel's make with enclosed box along with all its necessary 1 connection complete.(Anchor)	nos	808.00	2	1616.00
7	Earthing in soft soil with 50 mm dia GI pipe (TATA make Medium) 3.64 mm th. X 3.04 Mtr long and 1 x 4 SWG GI (hot dip) wire (4 m long) 13 mm dia x 80 mm long GI bolts, double nuts, double washer including S & F 15 mm dia GI protection (1 mtr long) to be filled with bitumen partly under the ground level & partly above GL driven to an average depth of 3.65 m below the GL & restoring surface duly rammed.	each	1715.00	1	1715.00



8	Connecting the equipment to earth BUSbar inclusive S&F 10 SWG (Hot Dip) GI wire on wall /floor with a staples buried inside wall /floor as required & making connection to equipments with bolt, nut, washer, cable lugs etc. as required & mending good damages.	M	6.00	5	30.00
			TOTAL		17858.00
	<b>Rupees Thirteen Thousand Eight Hundred Seventy Eight Only</b>				<b>17858.00</b>

Table-20: Cost Estimate for 2 Nos Leach Pit for single unit Dwelling Unit

Cost Estimate for 2 Nos Leach Pit for single unit Dwelling Unit P.W.D Schedule of Rates effect from 1st July 2014					
(ANNEXURE-II)					
Sl No	Description of Items	Quantity	Unit	Rate	Amount
1	Earth work in excavation of foundation trenches or drains in all sorts of soil (including mixed soil but excluding or stacking the spoils within a lead of 75 m. as directed. The item includes necessary trimming the sides of trenches leveling dressing and ramming the bttom boiling out water aqs required complete . Depth of exavation not existing 1500mm P.No-1, I-2(a)	2.500	%Cu.M	12047.00	301.18
2	Cement concrete with graded jhama Khoa ballast (30 mm size) excluding shuttering. In ground floor and foundation (a) 6:3:1 proportion.	0.050	Cu.M	5803.06	290.15
3	Brick work with 1st class bricks in cement mortar (6 :1). a) In foundation & Plinth P.no-29, I-21(a)	0.010	Cu.M	5719.00	57.19
4	125 mm. thick brick work with 1st class bricks in cement mortar (4 : 1) G.Floor P.no-31, I-29	3.000	SqM	714.00	2,142.00
5	Controlled Cement concrete with well graded stone chips (20 - mm nominal size) excluding shuttering and reinforcement with complete design of concrete as per I : 456 and relevant special publications submission of job mix formula after preliminary mlx design after testing of concrete cubes as per direction of Engineer-in charge Consumption of cement will not be less than 300 Kg of cement -with Super plasticiser per cubic meter of controlled concrete but actual consumption will be determined on- the basis of preliminary test and job mix formula. -I n ground floor and foundation. [Using concrete mixture] M	0.145	Cu.M	6871.54	996.37



	20 Grade <b>P.no-12, I-6(a)</b>				
6	Reinforcemnet for reinforced concrete work in all sorts of structures incl. Distribution bars, stirrups, binder etc. incl. supply of rods, initial straightening & removal of loose rust (if necessary), cutting to requisite length, hooking etc <b>P.no-27, I-15(a)(i)</b>	0.010	M.T	68508.00	685.08
7	Supplying, fitting and fixing UPVC down pipes A type and fittings conforming to IS 13592-1992 with necessary clamps nails including making holes in walls, etc. and cutting trenches in any soil, through masonry concrete structure etc. if necessary and mending good damages including jointing with jointing materials ( Spun yarn, valamoid / bitumen / M. seal etc.) complete.				
	i) UPVC Pipe 110 mm dia <b>P.no-173, I-21(A)(ii)</b>	4.000	Mtr	291.00	1,164.00
	ii) UPVC Bend 87.5 degree 110 mm dia <b>P.no-174, I-21(B)C(ii)</b>	2.000	Each	162.00	324.00
8	Jaffri brick work 125 mm. thick with 1st class bricks in cement mortar (4:1) including 12 mm. thick cement plaster (4:1) in all faces in ground floor <b>P.no-32, I-35</b>	2.000	SqM	792.00	1,584.00
Cost of 2 no leach pit					7,543.97
				Total=	7,544.00

Table-21: Detailed Estimate for Single Dwelling unit

Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partitionwall			Varandah C/L	
		4.65			3.375			1.275	
		0.8			1.15			0.9	
		1.15			1.15	2.3		2.175	
		3.45			2.187				
		1.15			1.9				
		1.7			1.387	5.474			
		3.375			11.149				
		1.275							
		2.825							
		3.125							



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partition wall			Varandah C/L	
		23.5							
	X wall	1.25							
Sl.no.									
1	Earth work in excavation								
	250 mm wall								
	1	23.5	0.75	0.7	12.34				
		0.875	0.75	0.7	0.46				
		24.375			12.8	m3			
	125 mm Wall								
		2.625	0.4	0.225	0.24				
	WC	0.4	0.4	0.225	0.04				
	Bath	0.65	0.4	0.225	0.06				
	5.474	0.75		0.225					
		4.724	0.4	0.225	0.43				
	Varanda	1.425	0.4	0.225	0.13				
					0.88				
	Step	0.5	0.9	0.075	0.034				
					13.715	m3			
2	Soling								
		24.375	0.75		18.281				
		11.45	0.4		4.58				
					22.861				
3	Polythene sheet								
		2.575	3.125		8.047				
		2.875	2.625		7.547				
		2	1.65		3.3				
	passage	0.625	2.375		1.484				
	Bath&WC	2.7	0.9		2.43				
	Varndah	1.025	0.6		0.615				
	step	0.9	0.5		0.45				
					23.873				
4	Jhama concrete								
			18.28	0.075	1.371				
			4.58	0.075	0.344				
			23.93	0.075	1.795				
					3.51				
5	Earth work in filling 1/5 excavation								
			13.715	5	2.743				
			23.48	0.375	8.805				



Detailed Estimate for Single Dwelling unit Floor area 25.34 sqm Built up area 32.58 sqm									
	C/L of main outer wall				125 mm Partition wall			Varandah C/L	
					11.548	m3			
6	B.W (6:1) in Foundation of plinth								
		23.5	0.625	14.6875					
		23.5	0.5	11.75					
		23.5	0.375	8.8125					
				35.25	0.15	5.288			
		23.5	0.25		0.525	3.084			
	X wall	0.938	0.625	0.586					
		1	0.5	0.5					
		1.063	0.375	0.399					
				1.485	0.15	0.223			
		1.125	0.25		0.525	0.148			
	125mm	3.125	0.25		0.525	0.41			
	Bath&WC	2	0.9	0.25	0.523	0.235			
	Kit	5.224	0.25		0.525	0.686			
	Vard	1.925	0.25		0.525	0.253			
	Steps	0.5	0.9		0.15	0.068			
		0.25	0.9		0.15	0.034			
						10.427	m3		
7	DPC	23.5							
		1.125							
		24.625		0.25		6.156			
		3.125							
		1.8							
		5.224							
		10.149		0.125		1.269			
						7.425			
	Less	0.9		0.25	0.225				
		0.9		0.125	0.113				
	3	0.75		0.125	0.281				
						0.619			
						6.806	sqm		
8	BW in super structure (6:1)								
		23.5							
		1.125							
		24.625	2.75	0.25	16.93				



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partition wall			Varandah C/L	
	Parapet	23.8	0.075	0.25	0.446				
						17.376			
	Less opens								
	1	0.9	2.1	1.89					
	4	0.9	0.9	3.24					
	1	0.75	0.9	0.675					
	3	0.75	0.75	1.688					
				7.493	0.25	1.873			
	Lintel								
	1	1.525	1.525						
	4	1.2	4.8						
	1	1.05	1.05						
			7.375	0.25	0.1	0.184			
	Wo2								
	1	3.05	3.05	0.25	0.1	0.076			
					(-)	2.134			
	Net brick work						15.242	m3	
9	125 th. Brick work (6:1)								
	room		3.125	2.6	8.125				
	kit		2.125	2.75	5.844				
			1.65	2.75	4.5375				
			1.45	2.65	3.8425				
	2		0.9	2.1	3.78				
						26.12875			
	Less opening								
	1	0.9	0.9						
	3	0.75	2.25						
			3.15	2.1	6.615				
	Lintel								
	1	1.3	1.3						
	1	1.025	1.025						
			2.325	0.1	0.2325				
					6.8475				
						19.28125			
	Parapet								
		23.5		0.15		3.525			
						22.806			
	passee	0.75		0.55		0.4125			



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partition wall			Varandah C/L	
						23.219	sqm		
10	Conc M-20								
	Roof slab								
	32.15	1.1475	31.003		0.1	3.1			
	Beam		3.625	0.25	0.15	0.136			
			2.575	0.25	0.1	0.064			
	Lintel						3.301		
	D1	1	1.525	1.525					
	W1	4	1.2	4.8					
	W2	1	1.05	1.05					
	WO2	1	3.05	3.05					
				10.425	0.25	0.1	0.261		
	D1	1	1.39	1.39					
	D2	1	1.025	1.025					
	D2	2	1.4	2.8					
	O2	1	0.875	0.875					
	D2	2		6.09	0.125	0.1	0.076		
	Chaja								
	W1	4	1.2	4.8					
	W2	1	1.03	1.03					
	D1	1	1.275	1.275					
	W02	1	3.05	3.05					
				10.155	0.3	0.075	0.228		
							3.866	m3	
11	Reinforcement								
		3.866	0.80%	1	7850	0.243	MT		
12	Shuttering								
	31	23.5	1.125						
			24.63	0.25					
	31			6.156	24.844				
	Side beam	2	3.125	0.15	0.9375				
		2	2.325	0.1	0.465				
	side slab	1	25.3	0.1	2.53				
	Lintel	1	0.9	0.25	0.225				
		1	1.525	0.1	0.153				
		1	1.275	0.35	0.446				



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partitionwall			Varandah C/L	
		1	0.3	0.05	0.015				
						29.615	sqm		
4W1		4	0.9	0.25	0.9				
		4	1.2	0.1	0.48				
		4	1.2	0.35	1.68				
	2	4	0.3	0.05	0.12				
1W2		1	0.75	0.25	0.188				
		1	1.05	0.1	0.105				
		1	1.05	0.35	0.368				
	2	1	0.3	0.05	0.03				
WO2		3	0.75	0.25	0.563				
	1	1	3.05	0.1	0.305				
		1	3.05	0.35	1.068				
	2	1	0.3	0.05	0.03				
Lintel 125 Wall									
D1		1	0.9	0.125	0.113				
		2	1.3	0.1	0.26				
D2		2	0.75	0.125	0.188				
	2	2	1.15	0.1	0.46				
D2		2	0.75	0.125	0.188				
		2	1.9	0.1	0.38				
						7.423			
						37.038	sqm		
13	Plaster (6:1)								
	Out side 15 mmth.								
			2.85	1.125	0.45				
		25.3			4.425	111.953	sqm		
	Inside 20 mm th.								
	2	2.7	3.125	2.75	32.038				
	2	2.875	2.625	2.75	30.25				
	2	2	1.65	2.75	20.075				
	2	2.075		2.75	11.413				
	Above lintel								
	1	0.75		0.65	0.488				
Bath									
	2	0.9		2.75	4.95				
WC									
	1	2.95		2.75	8.113				
	1	2.25		2.75	6.188				



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partition wall			Varandah C/L	
	4	2.2		0.9	7.92				
	T. 125 wall								
	2	0.9		0.125	0.225				
						121.658			
	Open out side less								
	3	0.75		2.1	4.725				
					(-)	4.725			
						116.933	sqm		
	Celling Plaster				24.47				
	Less				1.14				
						23.33	Sqm		
14	Neat cement punning								
	Out side	Plinth							
		25.3	0.45			11.385	Sqm	11.385	
	Inside		2.7	3.125					
		2		5.825	0.1	1.165	Sqm		
			2.875	2.625					
		2		5.5	0.1	1.1	Sqm		
	Kithen	2		1.65					
		2		3.65	0.45	3.285	Sqm		
		1		1.65	0.45	0.743	Sqm		
		2		2.075	0.1	0.415	Sqm		
	Varanda			1.775	0.1	0.178	Sqm		
	step WC	1		3	0.45	1.35	Sqm		
	Bath			3.5	2	7	Sqm		
				0.75	0.1	0.075	Sqm		
	In side punning						15.31	15.31	
	Total							26.695	Sqm
15	Art. Stone flooring								
	Floor area					25.37	sqm		
	Step	2	0.9	0.25		0.45			
	W1	4	0.9	0.1		0.36			
	W2	1	0.75	0.1		0.075			
	W3	3	0.75	0.1		0.225			
							26.48	Sqm	
16	Ms Clamp for door & window								
	D1+D2	4	6			24			



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partitionwall			Varandah C/L	
	W1+W2	5	2			10			
							34	nos.	
17	Wood work in Door & window frame								
	D1	2	5.1	10.2					
	D2	2	4.95	9.9					
	W1	4	3.6	14.4					
	W2	1	3.3	3.3					
				37.8	0.075	0.075	0.213	m3	
18	Z batten shutter								
	D1	2	0.775	2.025		3.139			
	D2	2	0.625	2.025		2.531			
	W1	4	0.775	0.775		2.403			
	W2	1	0.775	0.625		0.484			
							8.557	sqm	
19	Iron Butt Hinges								
	D1+D2					12			
	W1	4	4			16			
	W2	1	4			4			
							32	nos.	
20	Iron soket bolt								
	Door			6					
	Window			5					
							11	nos.	
21	White wash								
	Inside+Celling Plaster- inside punning								
			116.933	23.33	15.31		124.953	sqm	
22	Colour wash								
	Out side Plaster- out side punning								
			111.953	11.385			100.568	sqm	
23	Priming on timber sutrface								
	2	2	0.9	2.1		7.56			
	2	2	0.75	2.1		6.3			
	4	2	0.9	0.9		6.48			
	1	2	0.75	0.9		1.35			
							21.69	sqm	



Detailed Estimate for Single Dwelling unit Floor area 25.36 sqm Built up area 32.18 sqm									
	C/L of main outer wall				125 mm Partitionwall			Varandah C/L	
24	Painting best quality on wooden surface								
	same sl.no. 23						21.69	sqm	
25	MS ornamental gril... 10Kg-16 Kg								
	W1	4	0.75	0.75	2.25				
	W2	1	0.75	0.6	0.45				
					2.7				
					@12Kg/sqm		32.4	Kg	
26	Priming on Steel sutrface						2.7	sqm	
27	Painting best quality on steel surface						2.7	sqm	
	same sl.no. 24								
28	R.C.C. Shelf								
		1.75	0.5				0.875	sqm	
29	Roof treatment with cow dang								
				32.18					
	Deduct	1.14	(varanda)	1.14					
	Cornice	25	0.125	3.125					
				27.915			27.915	sqm	

## 5.2.2. Detailed Estimate of adoption of Concrete Road:

Table-22: Detailed Estimate of adoption of technology for Concrete

ESTIMATE FOR CONSTRUCTION OF CONCRETE ROAD 2.5 MRTRE WIDE								
PWD BUILDING SCHEDULE 2014								
Sl No	Description of Items	Length	Breadh	Depth	Quantity	Unit	Rate	Amount
1	Earth work in excavation of foundation trenches or drains in all sorts of soil (including mixed soil but excluding or stacking the spoils within a lead of 75 m. as directed. The item includes necessary trimming the sides of trenches leveling dressing and ramming the bttom boiling out water aqs required	1.00	2.5	0.40 0	1.000	%Cu. M	12047.00	120.47



ESTIMATE FOR CONSTRUCTION OF CONCRETE ROAD 2.5 MTRTRE WIDE								
PWD BUILDING SCHEDULE 2014								
Sl No	Description of Items	Length	Breadth	Depth	Quantity	Unit	Rate	Amount
	complete . Depth of exavation not existing 1500mm <b>P.No-1, I-2(a)</b>							
2	Filling foundation or plinth by silver sand in layer not exceeding 150 mm. as directed and consolidating same by through saturation with water rammingcomplete. Including the cost of supply of sand. (a) by fine sand <b>P.No-2 , I-4(B)</b>	1.00	2.5	0.20 0	0.500	%Cu. M	110422.0 0	552.11
3	Single brick flat soling of picked jhama bricks including ramming and dressing bed to proper level and filling joints with powdered earth or local sand <b>P.no-11, I-1</b>	1.00	2.5		2.500	Sq.M	377.00	942.50
4	Ordinary Cement concrete (mix 1:1.5:3) with graded stone chips (20 mm nominal size) excluding shuttering and reinforcement, if any, in ground floor as per relevant IS codes <b>P.no-24, I-10(a)</b>	1.00	2.5	0.12 5	0.313	Cu.M	6802.74	2,125.86
5	Brick edging 75 mm. wide with picked jhama bricks, laid true to line and level including cutting necessary trench in sopil or in hard metalled surface, laying the bricks and repacking the trench (on both sides of the edgeing) with spoils and ramming the same throughly, complete as per direction. (b) Brick-on-end edging (250 mm ) depth. <b>P.No-189 , I-3(b)</b>	2.00			2.000	%Mtr	9392.00	187.84
6	Removal of rubbish, earth etc. from the working site and disposal of the same beyond the compound in conformity with the Municipal /Corporation Rules forsuch disposal, loading into truck and cleaning the site in all respect as per direction of Engineer - in -Charge <b>P.no-9, I-13</b>	1.00	2.50 0	0.40 0	1.000	Cu.M	168.00	168.00
							<b>Total=</b>	<b>4,096.78</b>
							<b>Total=</b>	<b>4,097.00</b>



**Rate Analysis****Brick Work 4:1 in foundation & plinth**

Step - 1	Schedule Rate	Rs	6068.00(A)
Step - 2	Deduct cost of cement=(Quanty of cement)x(Issue rate of cement vide item no-1 column-4 Table 1-1 of Annexure-1 $0.055 \times 8100$	Rs	672.30(B)
Step - 3	Add cost of cement supplied by cost contractor including 10% proffite = $1.1 \times (\text{Quanty of cement}) \times (\text{Basik price of cement vide item no -1 column- 5 table-1-1 of annexure -1})$ $1.1 \times 0.055 \times 7364$	Rs	672.33 (C.)
	Note;- Quantity of cement shall be same as step-2 Final Rate of item = Rs A - Rs B + Rs C = Rs D	Rs	6068.03 (D)

**Rate Analysis****Ordinary Mix Concreate 1:1.5:3**

Step - 1	Schedule Rate	Rs	6802.63 (A)
Step - 2	Deduct cost of cement=(Quanty of cement)x(Issue rate of cement vide item no-1 column-4 Table 1-1 of Annexure-1 $0.286 \times 8100$	Rs	2316.6 (B)
Step - 3	Add cost of cement supplied by cost contractor including 10% proffite = $1.1 \times (\text{Quanty of cement}) \times (\text{Basik price of cement vide item no -1 column- 5 table-1-1 of annexure -1})$ $1.1 \times 0.286 \times 7364$	Rs	2316.71 (C.)
	Note;- Quantity of cement shall be same as step-2 Final Rate of item = Rs A - Rs B + Rs C = Rs D	Rs	6802.74 (D)

**Rate Analysis****P.C.C 1:3:6 With Jhama Khoa**

Step - 1	Schedule Rate	Rs	5803.00 (A)
Step - 2	Deduct cost of cement=(Quanty of cement)x(Issue rate of cement vide item no-1 column-4 Table 1-1 of Annexure-1 $0.16 \times 8100$	Rs	1296.00(B)
Step - 3	Add cost of cement supplied by cost contractor including 10% proffite = $1.1 \times (\text{Quanty of cement}) \times (\text{Basik price of cement vide item no -1 column- 5 table-1-1 of annexure -1})$ $1.1 \times 0.16 \times 7364$	Rs	1296.06 (C.)
	Note;- Quantity of cement shall be same as step-2 Final Rate of item = Rs A - Rs B + Rs C = Rs D	Rs	5803.06 (D)

<b>Annexure - II</b>				
<b>Format - A</b>				
(Format for Rate Analysis of Cement Concrete Item)				
Item 7. Ordinary Cement concrete (mix 1:1.5:3) with graded stone chips (20 mm nominal size) excluding shuttering and reinforcement if any, in ground floor as per relevant IS codes.				
(i) Pakur Variety				
Consumption of Stone aggregate ( Page B-59)	20 mm =	0.573	Cum	
	10 mm =	0.287	Cum	
Distance of site considered =		10	Km	
Steps	Quantity	Unit	Rate	Amount
Step - 1 Rate of item as per relevant section of this Schedule A =	1.00	CUM	5389.00	<b>5389.00</b>
Step - 2 Add cost of stone aggregate of different grading as per consumption required for one cum of concrete.				
( As per table:T-1)				
Station : kalyani				
20mm Nominal Size:	0.573	CUM	1463.00	838.30
10mm Nominal Size:	0.287	CUM	1296.00	371.95
Total B =				<b>1210.25</b>
Step - 3 Add cost of carriage of stone aggregate as per consumption required for one cum of concrete.				
( As per table:T-2)				
20mm Nominal Size:	0.573	CUM	178.50	102.28
10mm Nominal Size:	0.287	CUM	178.50	51.23
Total C =				<b>153.51</b>
Step - 4 Add cost for loading and unloading of stone aggregate				
( As per table:T-3)				
20mm Nominal Size:	0.573	CUM	58.00	33.23
10mm Nominal Size:	0.287	CUM	58.00	16.65
Total D =				<b>49.88</b>
Final Rate of Item = [Rs. A - Rs.B + Rs.C + Rs.D] = Rs.				<b>6802.64</b>

*man*  
SAE

*Sevan*  
Chairman  
Jiaganj-Azimganj Municipality



## **Section 6 – Project Implementation & Management Framework**

### **6.1. Institutional Framework for implementation**

#### **State Level Sanctioning and Monitoring Committee (SLSMC)**

##### **Indicative functions of SLSMC**

- Approval of Housing for All Plan of Action (HFAPoA)
- Approval of Annual Implementation Plan
- Approval of DPRs under various components of the Mission
- Approval of Annual Quality Monitoring Plans
- Reviewing progress of approved projects in the State and cities
- Monitoring of implementation of Mission
- Any other issues required for effective implementation of the Mission.

#### **Jiaganj-Azimganj Municipality**

- I. Jiaganj-Azimganj Municipality shall be the nodal agency for implementation of DPR under HFA and has set up a robust administrative structure for implementation. The roles and responsibilities of the key stakeholder are as follows:
- II. **Housing for All Nodal Officer:** Executive Officer of the Jiaganj-Azimganj Municipality has been designated as the HFA Nodal Officer for the Jiaganj-Azimganj Municipality demonstrating the commitment and willingness of the Jiaganj-Azimganj Municipality to implement the DPR under HFA
- III. **Housing for All Working Group:** Jiaganj-Azimganj Municipality has created a HFA working group with departmental heads of all key departments including PWD, Revenue, Health, Water Supply, Planning, Poverty and IHSDP. The working group was instrumental in preparing the DPR under HFA and going forward will be responsible for the implementation of DPR under HFA
- IV. **Slum level federation at city level and slum dweller association at slum level:** Jiaganj-Azimganj Municipality has two CDS covering 21 wards and plan to establish a slum level federation at city level and slum dweller association at slum level for smooth implementation of HFA and ensuring that the detailed project reports are prepared in consultation with the community. The slum dweller association would also implement the O&M plan, which community had agreed upon, by collecting the contributions amongst themselves and formation of group housing societies as may be required.

### **6.2. Implementation schedule**

1. Tendering and process for award of work must be completed within one month from the date approval of the Project.

2. Quarterly fund requirement to match the project schedule will be followed as per guideline of the State Government.
3. Slum-wise project delivery will be done within six months from the date approval of the Project.

### 6.3 Quarterly component wise investment schedule vis-a-vis means of finance (Central/State/ULB/Beneficiaries share)

**Table-24: Quarterly component wise investment schedule vis-a-vis means of finance  
(Central/State/ULB/Beneficiaries share)**

Fund Type	Total Project cost			DU for 1477 nos			Physical Infrastructure		
	DU for 1477 nos	Physical Infrastructure	Total	1st Quarter	2nd Quarter	Total	1st Quarter	2nd Quarter	Total
Central	2215.5	0	2215.5	886.2	886.2	714	0	0	0
State	2850.61	271.77	3122.38	1248.952	1248.952	455	135.885	135.885	271.77
ULB	0	271.77	271.77	108.708	108.708		135.885	135.885	271.77
Beneficiaries share	369.25	0	369.25	369.25	0	79	0	0	0
<b>Total</b>	<b>5435.36</b>	<b>543.54</b>	<b>5978.9</b>	<b>2613.11</b>	<b>2243.86</b>	<b>1248</b>	<b>271.77</b>	<b>271.77</b>	<b>543.54</b>

### 6.4. Monitoring mechanism at State, ULB and Community level.

Mission will be monitored at all three levels: City, State and Central Government. CSMC will monitor formulation of HFAPoA, Annual Implementation Plans (AIPs) and project implementation. Suitable monitoring mechanisms will be developed by the Mission. States and cities will also be required to develop monitoring mechanism for monitoring the progress of mission and its different components.

### 6.5. Quality Control & Quality Assurance Plan.

The implementation and management arrangement should mention the role of the State Level Nodal Agency (SLNA), State Level Technical Cell (SLTC), City Level Mission Directorate, City Level Technical Cell (CLTC) and Project Management Consultant (PMC).)

## Section 7 – Operation & Maintenance Plan

The Road needs to be maintained. It is proposed that operation and maintenance and servicing of these roads should be done by the Municipality. The Bustee Working Committee shall be the first level of responsibility for ensuring that the pipelines etc. are kept in good order. The project cell of the Municipality shall carry out the overall operation and maintenance.



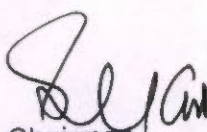
## Section 8 – Project Financials

Table-25: Project Financials

Component	Central share	State share	ULB share	Beneficiary Share	Total project cost
Housing	2215.50	2850.61		369.25	5435.36
Infrastructure	0.00	271.77	271.77	0.00	543.54
*O&M charges	0.00	0.00	0.00	0.00	0.00
*DPR Preparation, PM, TPIM, Social Audit Charges	0.00	3.80	0.00	0.00	3.80
Others	0.00	0.00	0.00	0.00	0.00
<b>Total</b>	<b>2215.50</b>	<b>3126.18</b>	<b>271.77</b>	<b>369.25</b>	<b>5982.70</b>

## Future Provision for construction of Housing

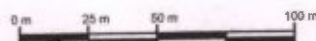
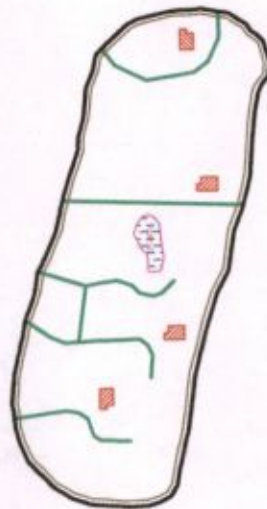
The poor people, who are residing on the land of Railway, the housing will be constructed on the railway land by Jiaganj-Azimganj Municipality if the Railway Dept. Govt. of India gives any permission.

  
Chairman  
Jiaganj-Azimganj Municipality





**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 01**  
**DHAKA PARA**  
**SLUM NO:- 001**  
**AREA:- 10000.00 SQM.**



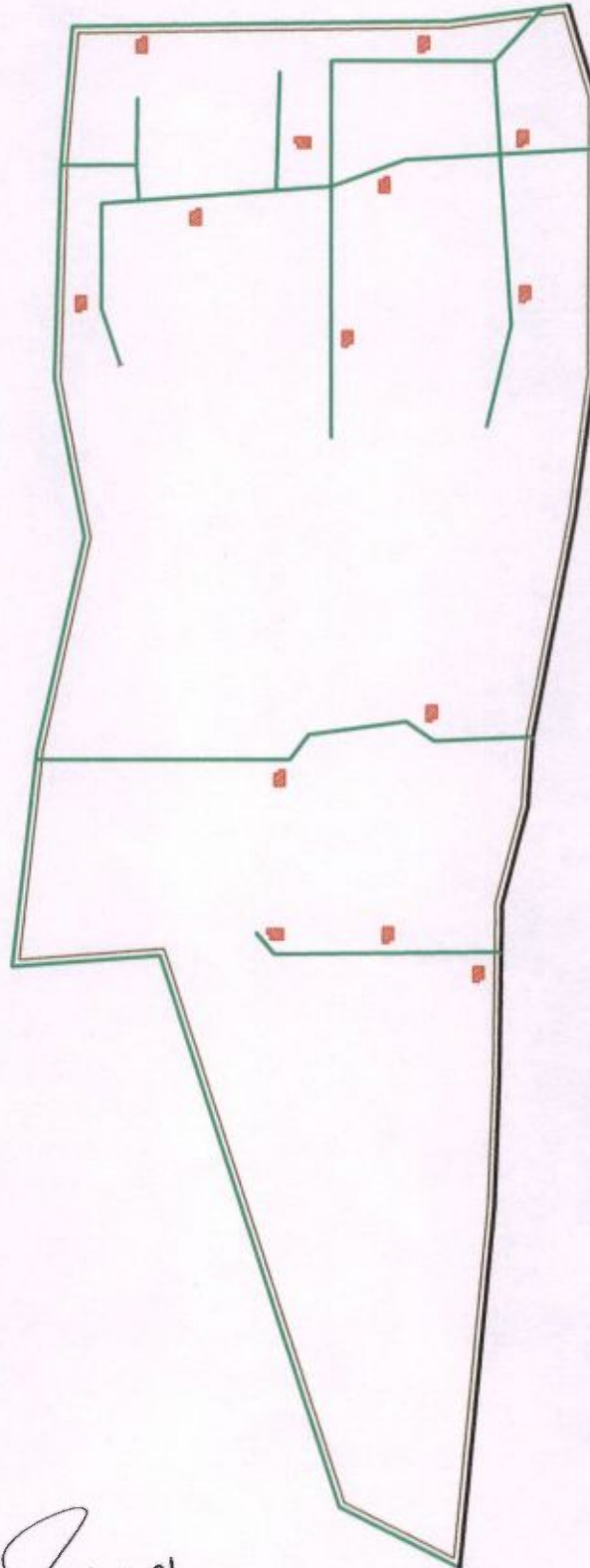
*Sankar Mandal*  
Chairman  
Jiaganj-Azimganj Municipality

PROPOSED LAND USE			
AREA OF SLUM :		SQM.	
POPULATION :		184	
CLIENT :		JIAGANJ-AZIMGANJ MUNICIPALITY	
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			4 nos
BLACK TOPPED ROAD			437.61 M.
CONCRETE ROAD			315.89 M.
WATER CONNECTION			4 nos



**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO - 1**  
**NUTON COLONY BUSTEE**  
**SLUM NO :- 002**  
**AREA :- 130000.00 SQM.**

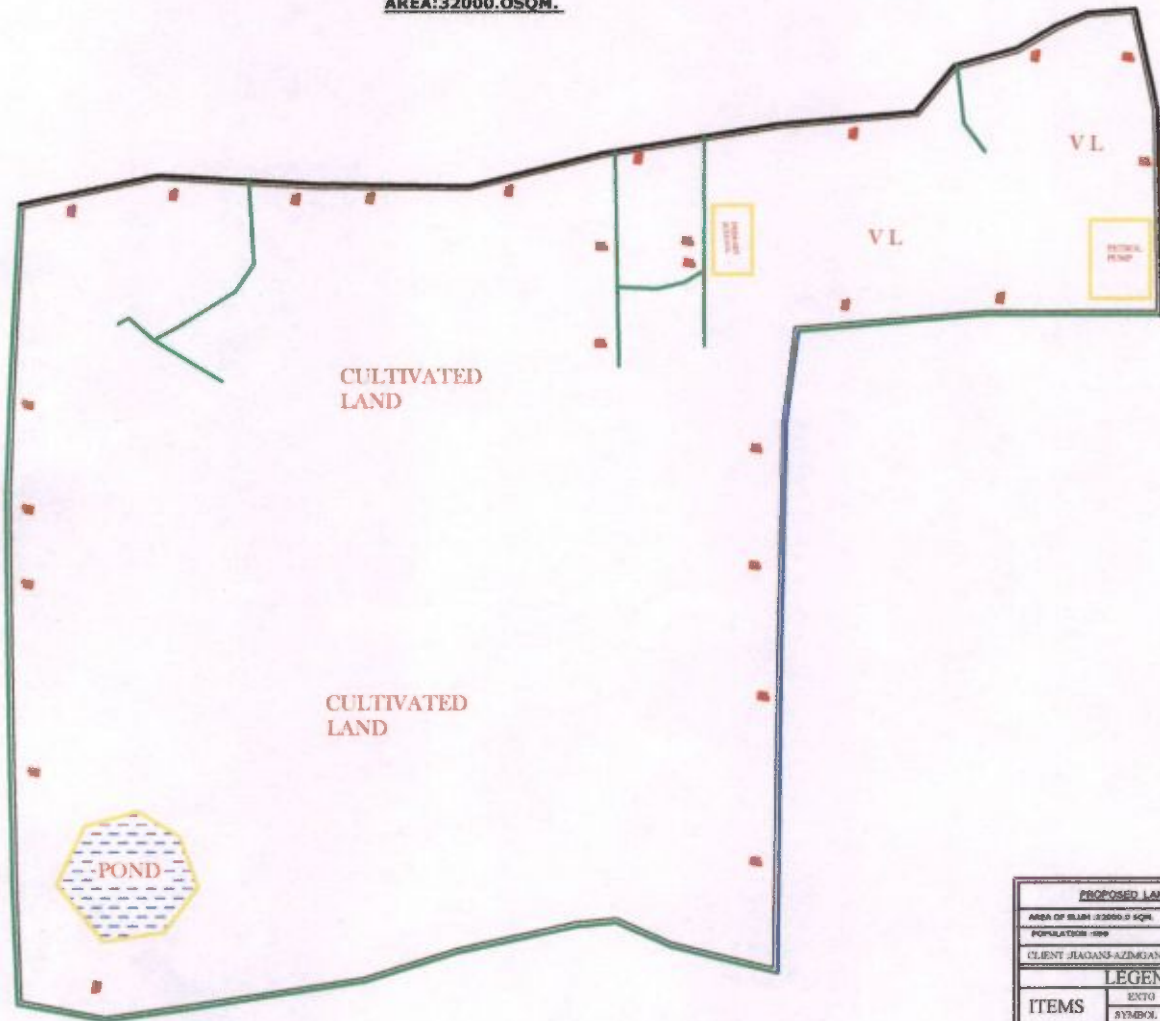


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Chairman  
Jiaganj-Azimganj Municipality

PROPOSED LAND USE			
AREA OF SLUM: 130000.00 SQM.			
POPULATION : 654			
CLIENT : JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			14 nos
BLACK TOPPED ROAD			741.22M.
CONCRETE ROAD			1996.73M.
WATER CONNECTION			14 nos

**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO-01  
PROMOD COLONY  
SLUM NO:- 003  
AREA:32000.0SQM.**



PROPOSED LAND USE			
AREA OF SLUM : 32000.0 SQM			
POPULATION : 1000			
CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXIST		PROPOSED
	SYMBOL	SYMBOL	QTY
WELL/SHED/STATION			1000
BLACK TOPPED ROAD			1000
UNPAVED ROAD			1000
WATER CONNECTION			1000

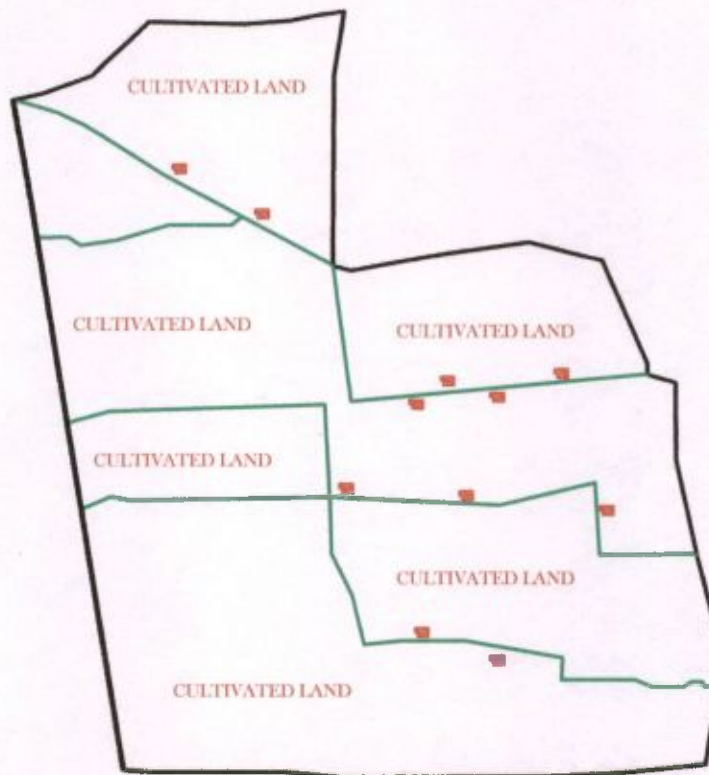
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Chairman  
Sankar Mandal  
Jiaganj-Azimganj Municipality







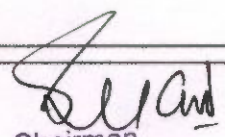
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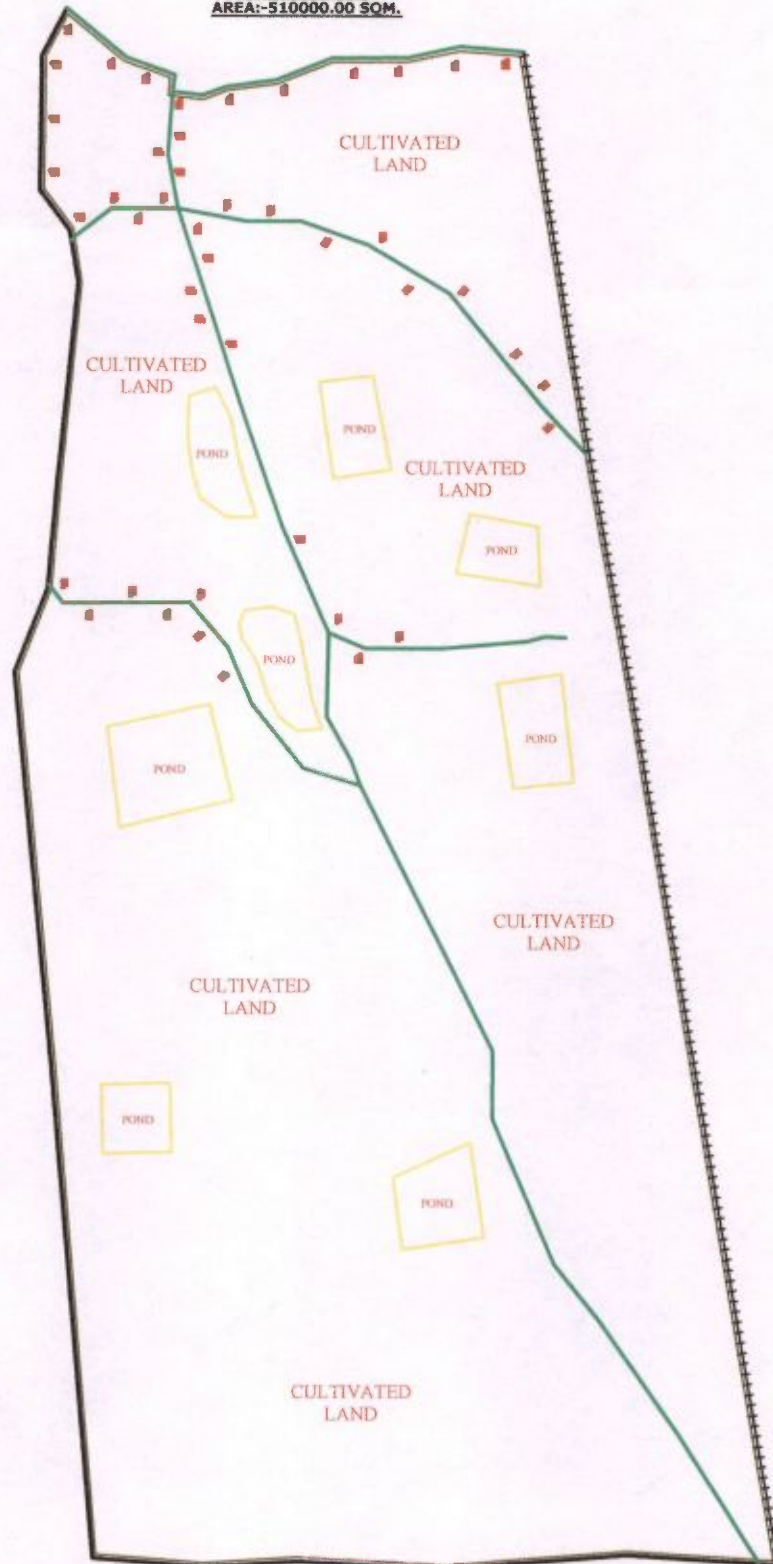
**WARD NO - 01**  
**PAHARIA PARA BUSTEE**  
**SLUM NO:- 004**  
**AREA:- 1730000.0 SQM.**



PROPOSED LAND USE			
AREA OF SLUM :		<u>SQM.</u>	
POPULATION :388			
CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PRO.	QUT.
	SYMBOL	SYMBOL	
DWELLINGHOUSE			11 NOS
CONCRETE ROAD			752.67 M
BLACK TOPPED ROAD			4331.25M
WATER CONNECTION			11 NOS

  
Chairman  
Sarkar Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**  
**ENATULIBAGH BUSTEE**  
**WARD NO - 1**  
**SLUM NO:- 005**  
**AREA:-510000.00 SQM.**



PROPOSED LAND USE			
Area of land - 510000.00 sqm			
Prepared by: 10/10/2019			
Client: JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXIST	PROPOSED	
EXISTING ROAD	SYMBOL	SYMBOL	QTY
NEW/UPGRADED ROAD	SYMBOL	SYMBOL	QTY
EXISTING BUILDING	SYMBOL	SYMBOL	QTY
NEW/UPGRADED BUILDING	SYMBOL	SYMBOL	QTY

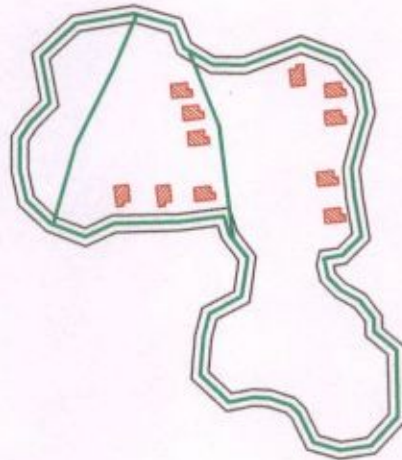
*Sarkar Mandal*  
 Chairman  
 Sarkar Mandal  
 Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 02**  
**MAHAJAN PATTY**  
**SLUM NO:- S.C-006**  
**AREA:- 10000.0 SQM.**



**PROPOSED LAND USE**

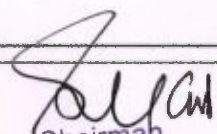
AREA OF SLUM 1 **SQM.**

POPULATION : 158

CLIENT : JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			11 nos
BLACK TOPPED ROAD			00.0 M.
CONCRETE ROAD			699.68M.
WATER CONNECTION			11 nos

  
Chairman  
Sarika Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 04**  
**BHALLADANGA-1 PARA BUSTEE**

**SLUM NO:- 007**  
**AREA:- 40000.0 SQM.**



**PROPOSED LAND USE**

AREA OF SLUM : **SQM.**

POPULATION : 107

CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			2 nos
BLACK TOPPED ROAD			746.00 M.
CONCRETE ROAD			480.00 M.
WATER CONNECTION			2 nos

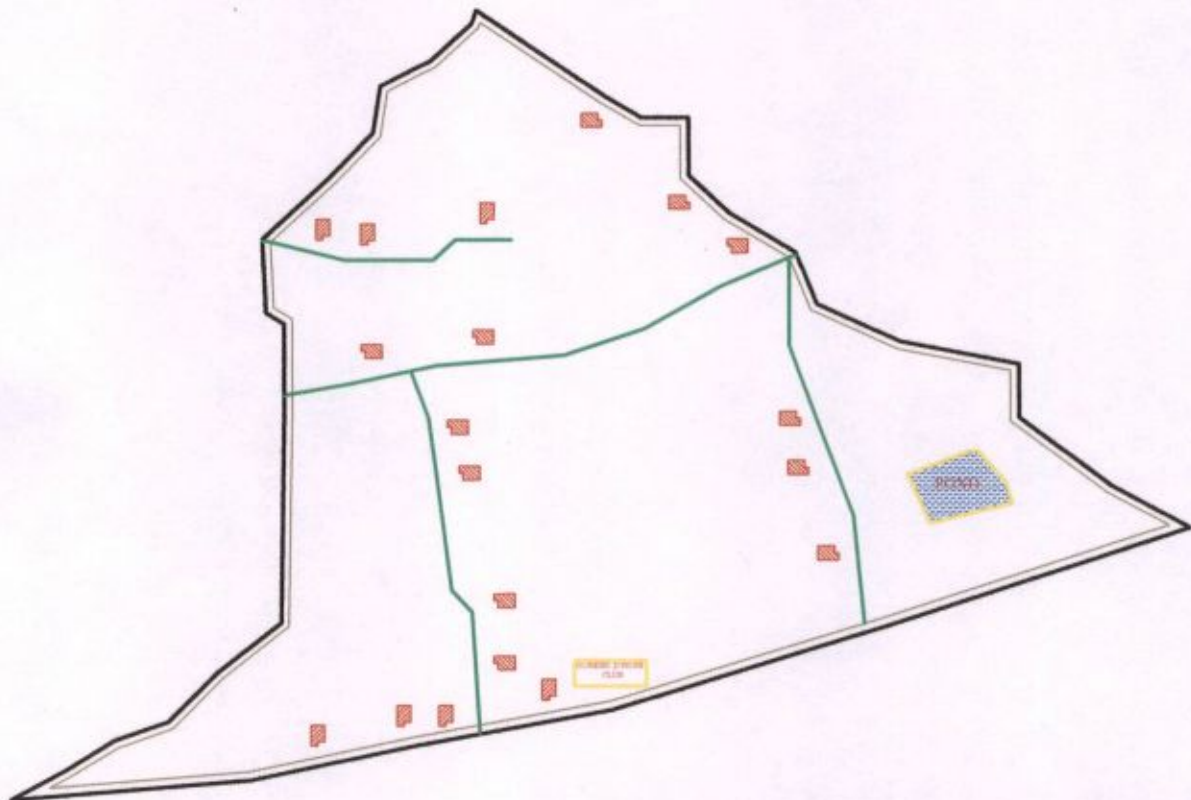
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Sarkar Mandal  
Jiaganj-Azimganj Municipality






**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 03**  
**SAMBHUNAGAR COLONY BUSTEE**  
**SLUM NO:- 0008**  
**AREA:- 50000.0 SQM.**



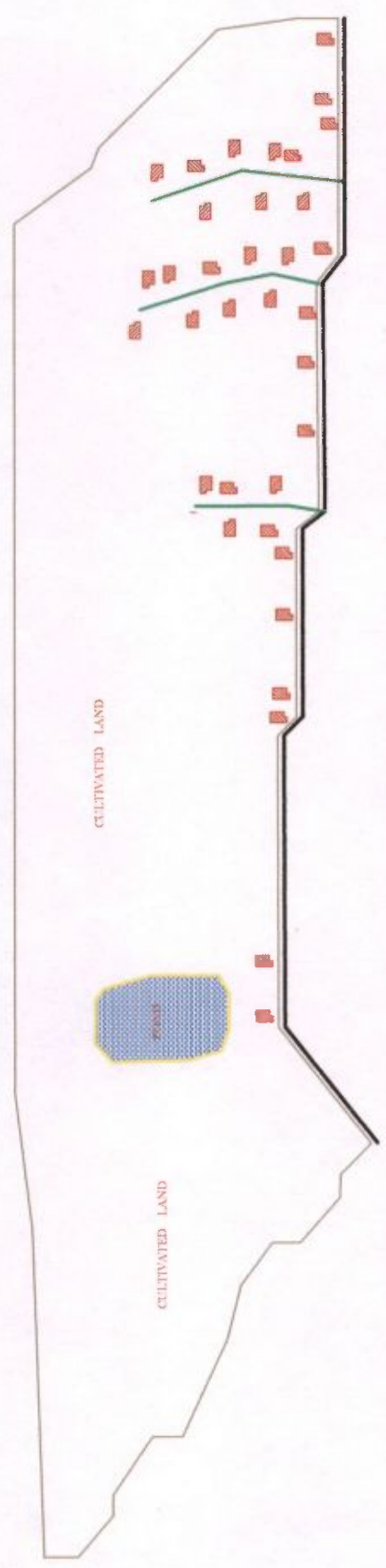
PROPOSED LAND USE			
AREA OF SLUM :		<u>SQM.</u>	
POPULATION :		353	
CLIENT :		JIAGANJ-AZIMGANJ MUNICIPALITY	
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			19nos
BLACK TOPPED ROAD			1172.0 M.
CONCRETE ROAD			460.3 M.
WATER CONNECTION			19 nos




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Sankar Mandal  
Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO - 03**  
**CHAI PARA BUSTEE**  
**SLUM NO:- 009**  
**AREA:- 80000.0 SQM.**



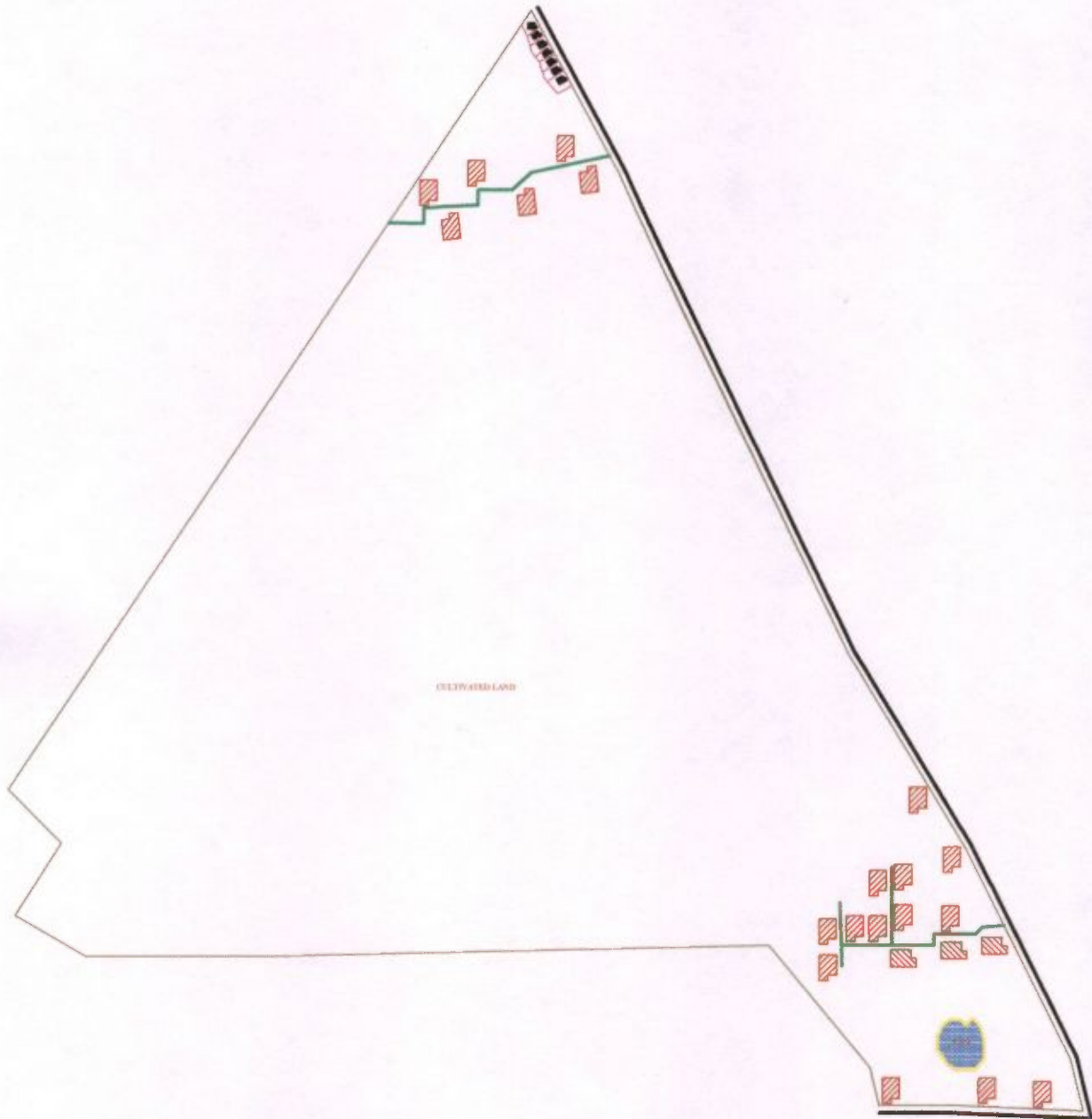
PROPOSED LAND USE			
AREA OF SLUM :		SQM.	
POPULATION : 422			
CLIENT : JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
		SYMBOL	QTY
DWELLINGHOUSE			35nos
BLACK TOPPED ROAD			540.0 M
CONCRETE ROAD			229.0 M
WATER CONNECTION			35 nos

  
Chairman  
Sarfaraz Mandal  
Jiaganj-Azimganj Municipality



**JIAGANJ-AZINGANJ MUNICIPALITY**

**WARD NO - 03  
STATION PARA BUSTEE  
SLUM NO:- 010  
AREA:- 420000.0 SQM.**

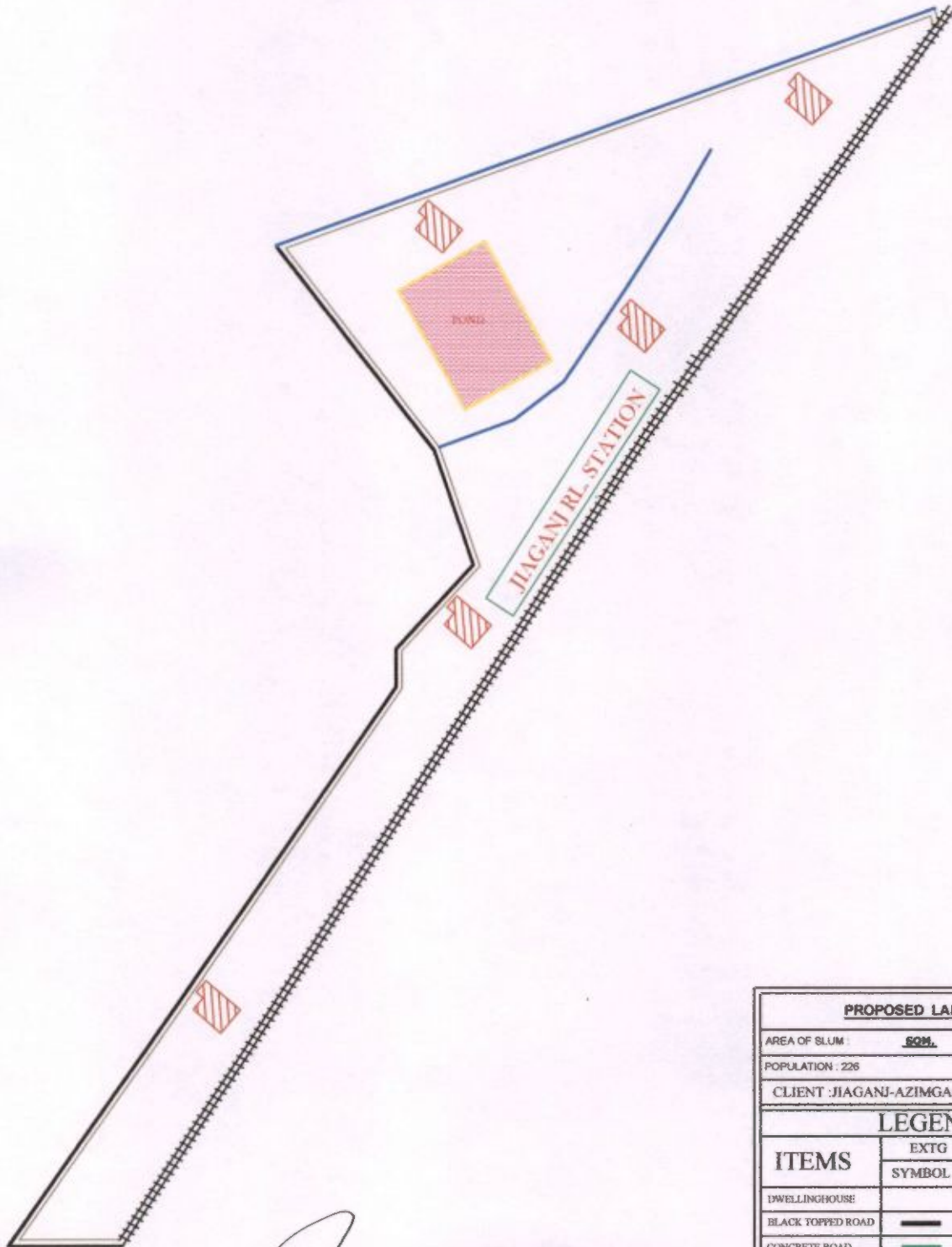






PROPOSED LAND USE			
AREA OF SLUM			
POPULATION 407			
CLIENT JIAGANJ-AZINGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXIST	PROPOSED	
	SYMBOL	SYMBOL	QTY
OPEN SPACE			20.00
BLACK TOPPED ROAD			100.00
CONCRETE ROAD			100.00
WATER CONDUCTION			10.00

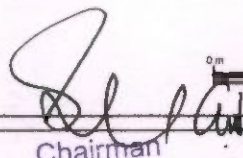
*Sankar Mandal*  
Chairman  
Jiaganj-Azinganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO - 03**  
**BHAGAN PARA BUSTEE**  
**SLUM NO:- 011**  
**AREA:- 50000.0 SQM.**



PROPOSED LAND USE			
AREA OF SLUM :		<u>SQM.</u>	
POPULATION :		226	
CLIENT :		JIAGANJ-AZIMGANJ MUNICIPALITY	
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			5 nos
BLACK TOPPED ROAD			666.18 M
CONCRETE ROAD			557.4 M
WATER CONNECTION			5 nos

  
Chairman

Sarfaraz Mardal  
Jiaganj-Azimganj Municipality







**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 04**  
**GUHA PARA BUSTEE**  
**SLUM NO:- 012**  
**AREA:- 170000.0 SQM.**



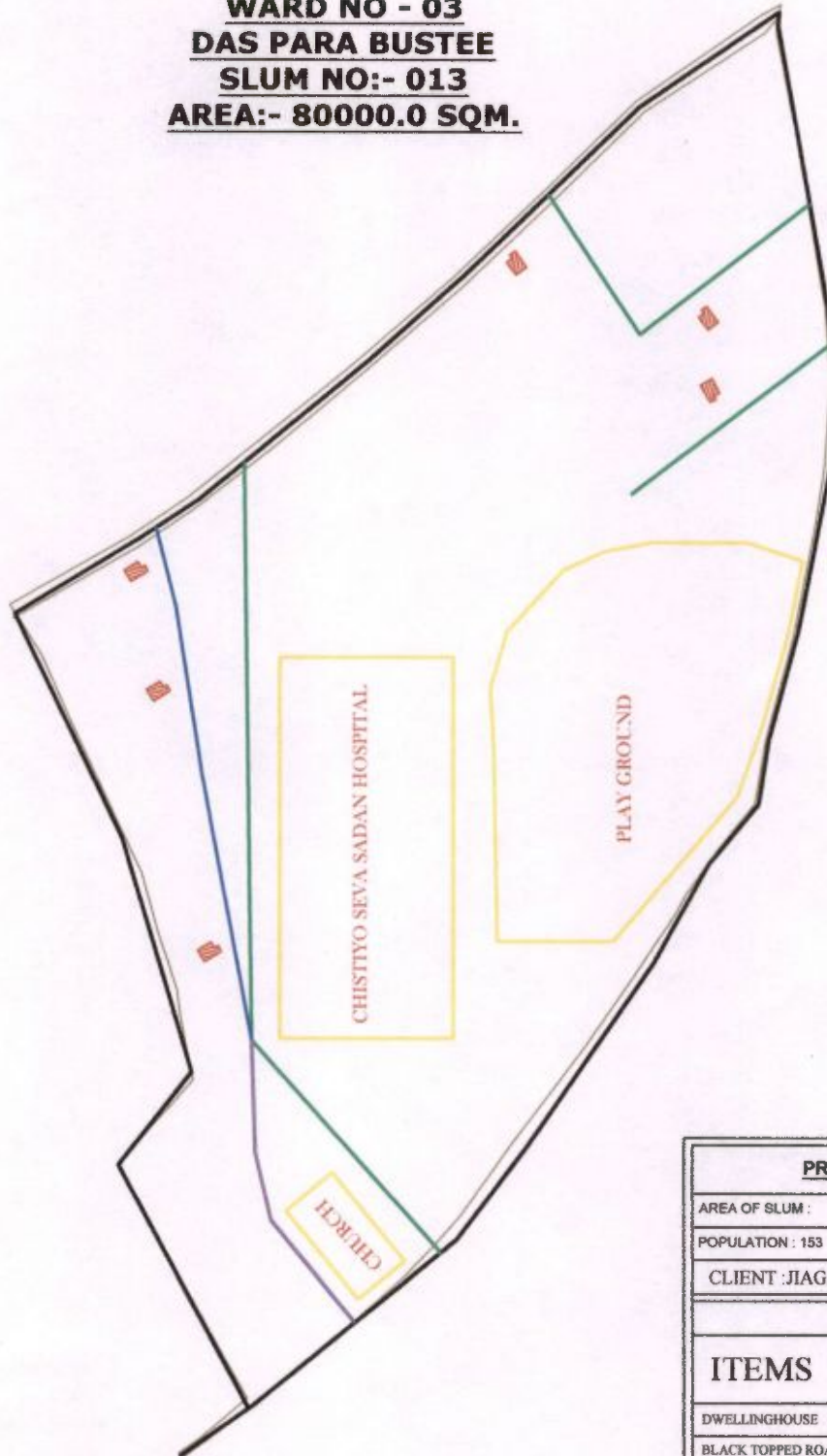
*Sankar Mandal*  
Chairman  
Jiaganj-Azimganj Municipality

PROPOSED LAND USE			
AREA OF SLUM :		20M.	
POPULATION : 107			
CLIENT JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			15nos
BLACK TOPPED ROAD			1579.00 M
CONCRETE ROAD			480.00 M
WATER CONNECTION			15 nos

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 03**  
**DAS PARA BUSTEE**  
**SLUM NO:- 013**  
**AREA:- 80000.0 SQM.**



0 m 25 m 50 m 100 m

**PROPOSED LAND USE**

AREA OF SLUM : **SQM.**

POPULATION : 153

CLIENT : JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			6 nos
BLACK TOPPED ROAD			1330.06 M
CONCRETE ROAD			872.83 M
WATER CONNECTION			6 nos

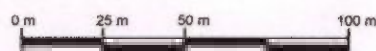
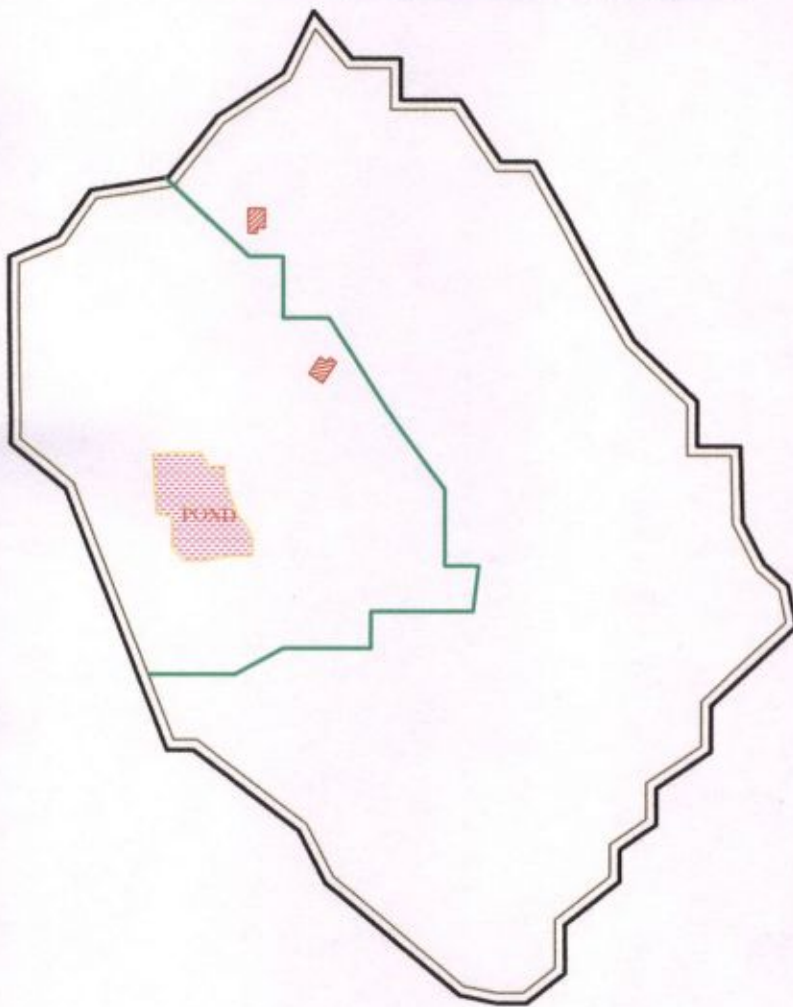
Chairman  
Sankar Mandal  
Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 03**  
**BHALLADANGA-2 BUSTEE**  
**SLUM NO:- 014**  
**AREA:- 40000.0 SQM.**



**PROPOSED LAND USE**

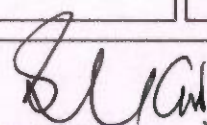
AREA OF SLUM : **SQM.**

POPULATION : 446

CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY

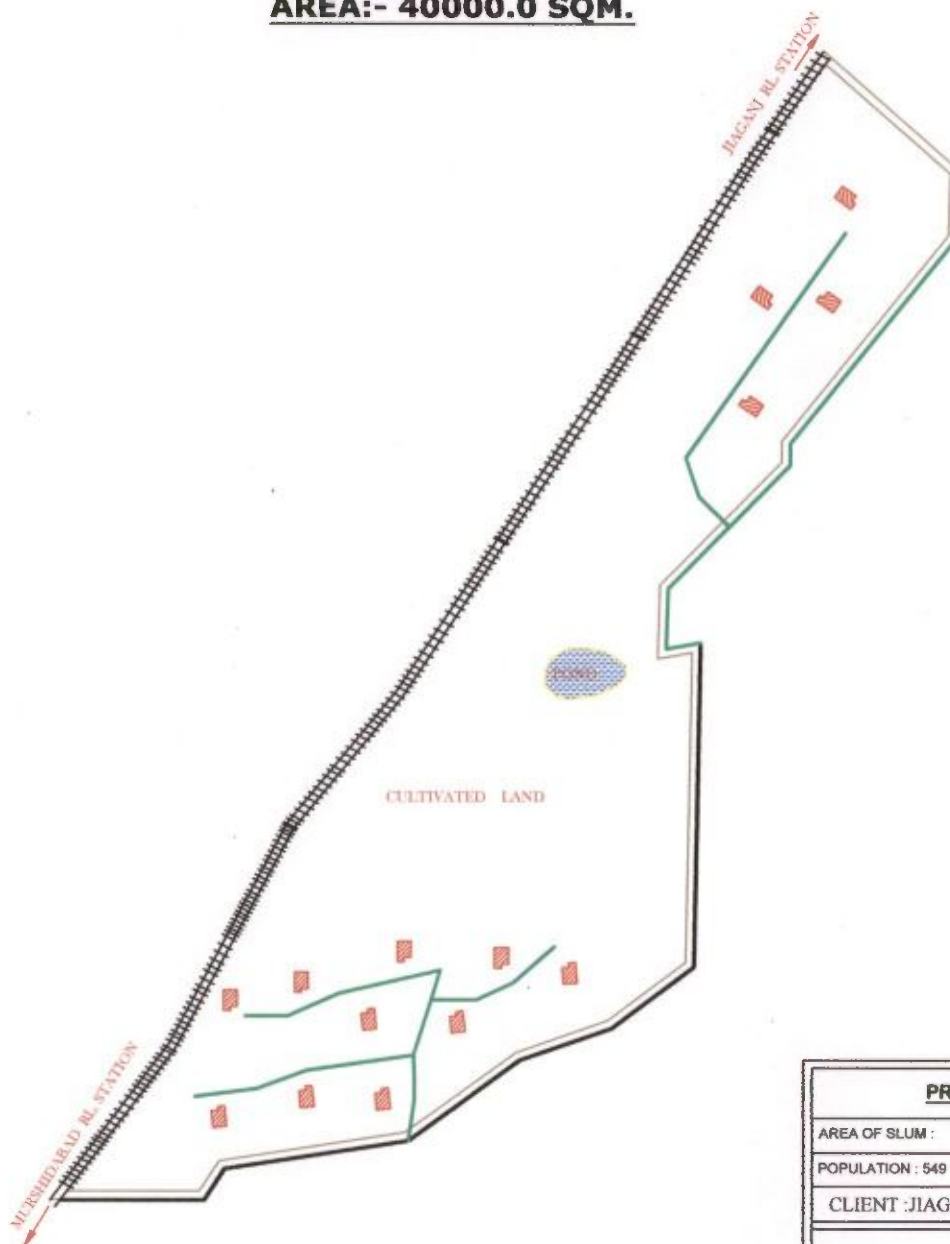
**LEGEND**




ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			2 NOS
BLACK TOPPED ROAD			859.00M.
CONCRETE ROAD			320.19M.
WATER CONNECTION			2 NOS

  
Chairman  
Sarfaraz Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO - 03**  
**BHALLADANGA-3 BUSTEE**  
**SLUM NO:- 015**  
**AREA:- 40000.0 SQM.**



PROPOSED LAND USE			
AREA OF SLUM :		<u>SQM.</u>	
POPULATION :		549	
CLIENT :		JIAGANJ-AZIMGANJ MUNICIPALITY	
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			14nos
BLACK TOPPED ROAD			374.0 M.
CONCRETE ROAD			400.0 M.
WATER CONNECTION			14 nos

0 m 25 m 50 m 100 m

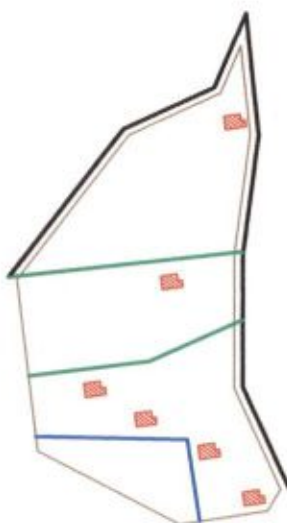
  
Chairman  
Sarkar Mandal  
Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 04**  
**POST-OFFICE PARA BUSTEE**  
**SLUM NO:- 016**  
**AREA:- 10000.0 SQM.**



0 m 25 m 50 m 100 m

  
Chairman  
Sarfaraz Mandal  
Jiaganj-Azimganj Municipality




**PROPOSED LAND USE**

AREA OF SLUM : 50M

POPULATION : 174

CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			6nos
BLACK TOPPED ROAD			311.0 M.
CONCRETE ROAD			167.44 M.
WATER CONNECTION			6 nos

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 04**  
**TANTI PARA BUSTEE**  
**SLUM NO:- 017**  
**AREA:- 30000.0 SQM.**



*Sarkar Mandal*  
Chairman  
Sarkar Mandal  
Azimganj Municipality

**PROPOSED LAND USE**

AREA OF SLUM : **SQM.**

POPULATION : 300

CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			7 nos
BLACK TOPPED ROAD			769.0 M.
CONCRETE ROAD			330.0 M.
WATER CONNECTION			7 nos






**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 04**  
**MORICHA PARA BUSTEE**  
**SLUM NO:- 018**  
**AREA:- 40000.0 SQM.**



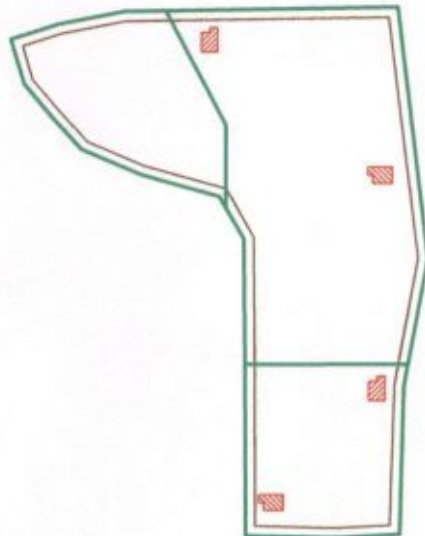
PROPOSED LAND USE			
AREA OF SLUM :		<u>SQM.</u>	
POPULATION :		433	
CLIENT :		JIAGANJ-AZIMGANJ MUNICIPALITY	
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			15nos
BLACK TOPPED ROAD			
CONCRETE ROAD			584.0 M.
WATER CONNECTION			15 nos

Chairman  
Sarkar Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 5**  
**HARIJAN PALLY BUSTEE**  
**SLUM NO:- 019**  
**AREA:-10000.00 SQM.**



**PROPOSED LAND USE**

AREA OF SLUM : 10000.00 SQM.

POPULATION : 323

CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			4 nos
BLACK TOPPED ROAD			00.00M.
CONCRETE ROAD			641.64M.
WATER CONNECTION			4 nos

0 m 25 m 50 m 100 m

Chairman  
Sarfaraz Mandal  
Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 06**  
**SIBTALLA GHAT BUSTEE**  
**SLUM NO:- 020**  
**AREA:- 20000.0 SQM.**



**PROPOSED LAND USE**

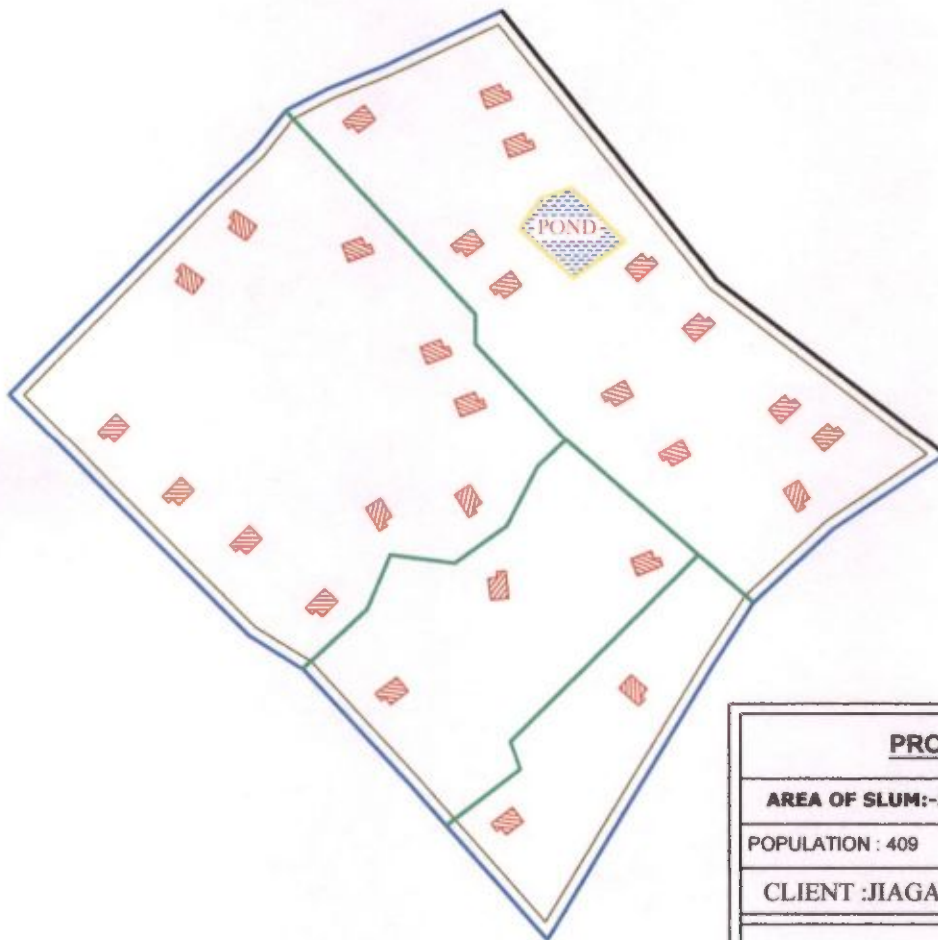
AREA OF SLUM : **SQM.**  
POPULATION : 359  
CLIENT : JIAGANJ-AZIMGANJ MUNICIPALITY

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			16nos
BLACK TOPPED ROAD			612.37 M.
CONCRETE ROAD			644.43 M.
WATER CONNECTION			16 nos

Chairman  
Sankar Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**  
**WARD NO-07**  
**GURI PARA BUSTEE**  
**SLUM NO:-021**  
**AREA:-30000.00 SQM.**



**PROPOSED LAND USE**

**AREA OF SLUM:-30000.00 SQM.**

**POPULATION : 409**

**CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY**

**LEGEND**

ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			28 nos
BLACK TOPPED ROAD			176.00M.
CONCRETE ROAD			974.37 M.
WATER CONNECTION			28 nos

Chairman

*Sarfaraz Mandal*

Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**

**WARD NO-07**

**SAMSAN PARA BUSTEE**

**SLUM NO:-022**

**AREA:-30000.00 SQM.**



**PROPOSED LAND USE**

**AREA OF SLUM:-30000.00 SQM.**

**POPULATION : 321**

**CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY**

**LEGEND**

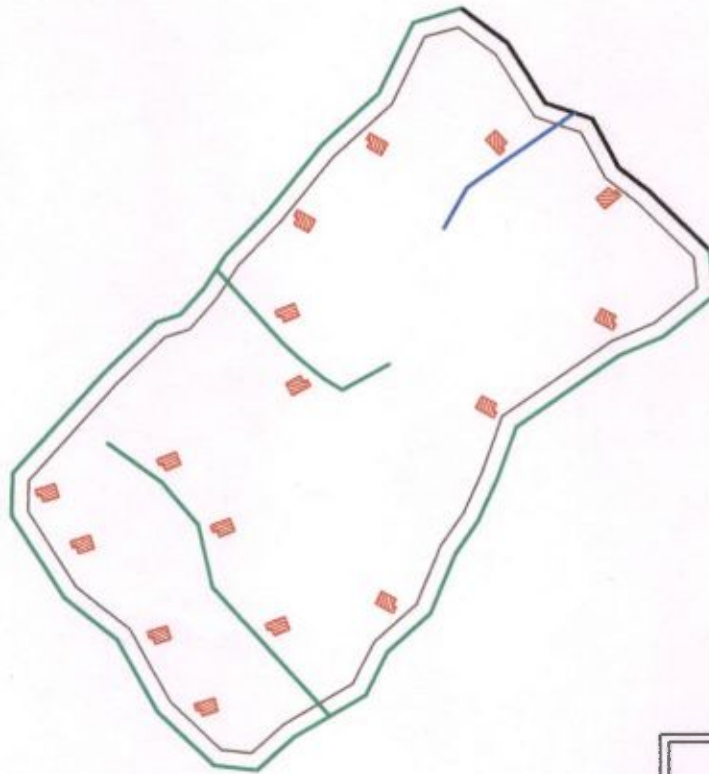
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			250rs
BLACK TOPPED ROAD			151.00M.
CONCRETE ROAD			896.15 M
WATER CONNECTION			25 nos

Chairman  
Sarkar Mandal  
Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO-07**  
**BANDHPUL**  
**SLUM NO:-023**  
**AREA:30000.0 SQM.**



PROPOSED LAND USE			
AREA OF SLUM :30000.0 SQM.			
POPULATION :467			
CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			16 nos
BLACK TOPPED ROAD			129.58 M.
CONCRETE ROAD			933.63M.
WATER CONNECTION			16 nos

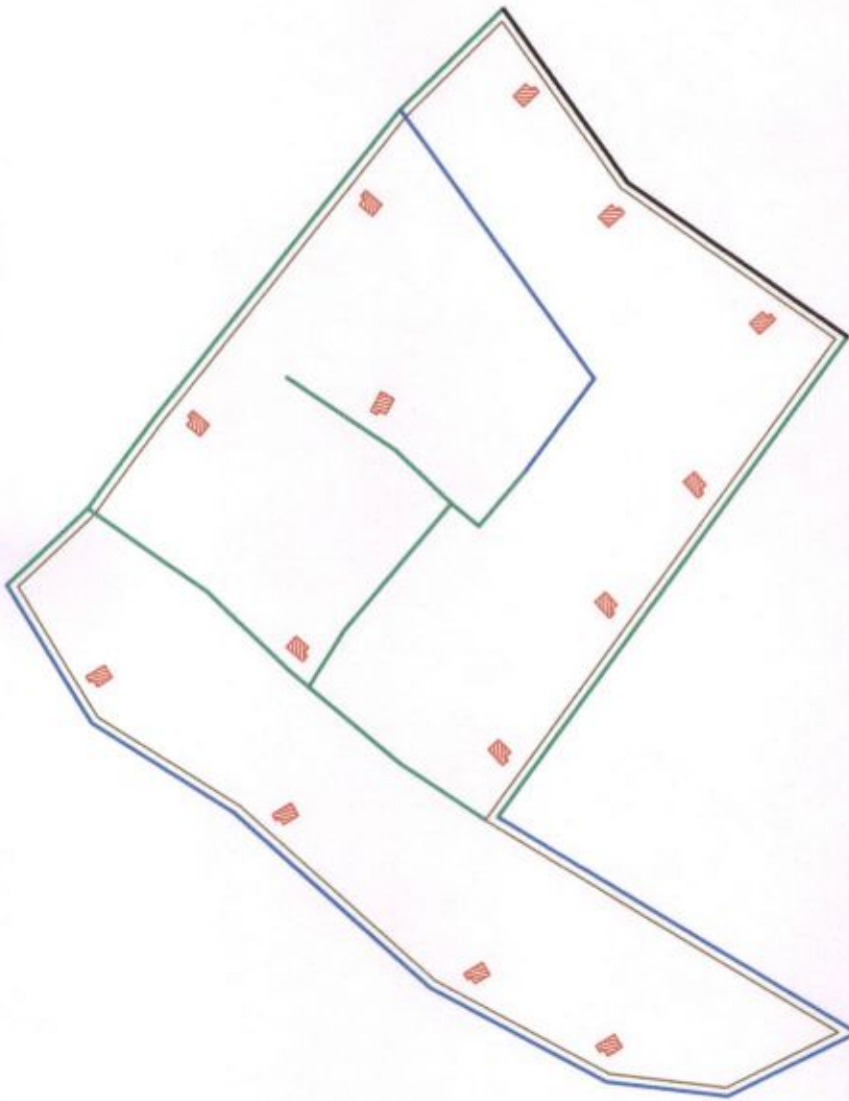
Chairman

*Sarfaraz Mandal*

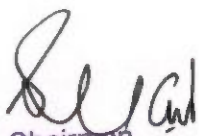
Jiaganj-Azimganj Municipality



**JIAGANJ-AZIMGANJ MUNICIPALITY**  
**WARD NO-07**  
**PULDERDHAR BUSTEE**  
**SLUM NO:-024**  
**AREA:-50000.00 SQM.**



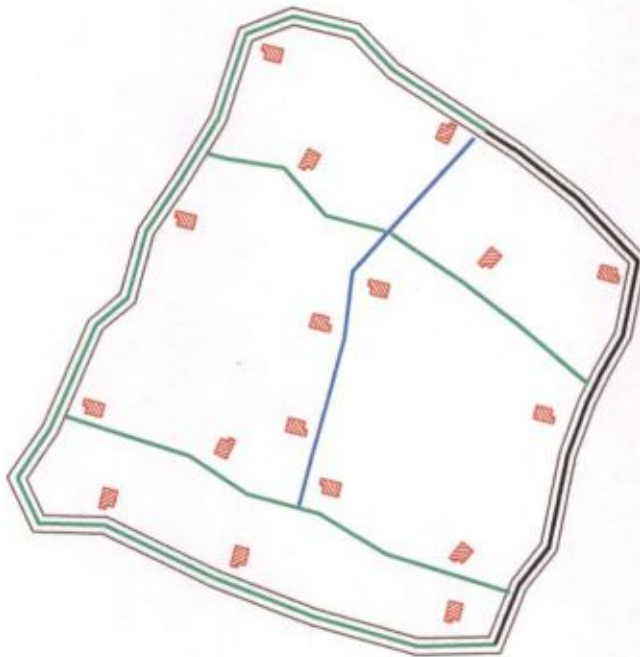
PROPOSED LAND USE			
AREA OF SLUM:-50000.00 SQM.			
POPULATION : 639			
CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLING HOUSE			14 nos
BLACK TOPPED ROAD			166.62M.
CONCRETE ROAD			1497.68M
WATER CONNECTION			14 nos



  
 Chairman  
 Sankar Mandal  
 Jiaganj-Azimganj Municipality

**JIAGANJ-AZIMGANJ MUNICIPALITY**



**WARD NO - 07**  
**KHANSPARA**  
**SLUM NO:- S.C-025**  
**AREA:30000.0 SQM.**



PROPOSED LAND USE			
AREA OF SLUM -3		<u>SQM.</u>	
POPULATION : 425			
CLIENT :JIAGANJ-AZIMGANJ MUNICIPALITY			
LEGEND			
ITEMS	EXTG	PROPOSED	
	SYMBOL	SYMBOL	QTY
DWELLINGHOUSE			17nos
BLACK TOPPED ROAD			223.20 M.
CONCRETE ROAD			960.90M.
WATER CONNECTION			17 nos

  
Chairman  
Sarkar Mandal  
Jiaganj-Azimganj Municipality